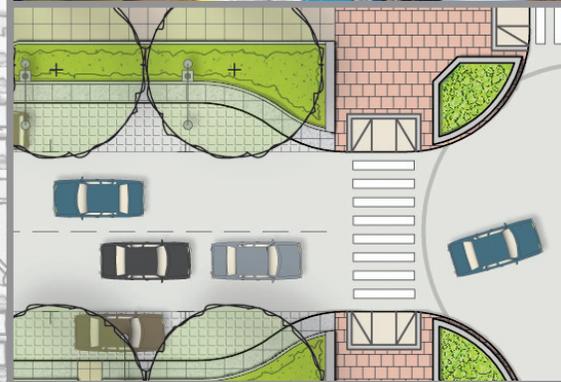


# DOWNTOWN SPECIFIC PLAN

THE CITY OF CERES



January 26, 2011





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FOR THE CITY OF CERES



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**DESIGN, COMMUNITY & ENVIRONMENT**

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# INTRODUCTION 1

The City of Ceres first began to develop in the 1870s in what is presently known as Downtown. Daniel Whitmore, the town's first resident, built a home on Fifth Street after he came to Ceres hoping for economic gain. Soon after Whitmore arrived, others began to settle the area, opening businesses around Fourth Street and along what is today known as State Route 99.

The City has since expanded its boundaries and increased significantly in population, particularly during the 1980s, 1990s and 2000s. Downtown remains one of the potential primary commercial and civic areas in Ceres, but other commercial areas, such as Mitchell Road, have lessened its economic significance within Ceres. In addition to the impact of citywide growth on the Downtown economy, State Route 99's emergence as a major regional connector has had a profound impact on the physical urban character and function of Downtown.

Downtown is currently home to a mix of neighborhood-serving businesses, offices and civic buildings, all surrounded by residential neighborhoods. Despite Ceres' recent growth, Downtown has experienced relatively little investment, particularly from the private sector. In 2007, a citywide strategic planning process was undertaken to begin formulating a vision for the City of Ceres. One of the cornerstones of that vision was the resurgence of Downtown as a key destination point. As a next step, the City decided to undertake a comprehensive community visioning and planning process focused specifically on Downtown. The result of these initiatives is this Specific Plan, which describes and illustrates a clear vision for the area and provides a policy and regulatory framework for achieving this vision.

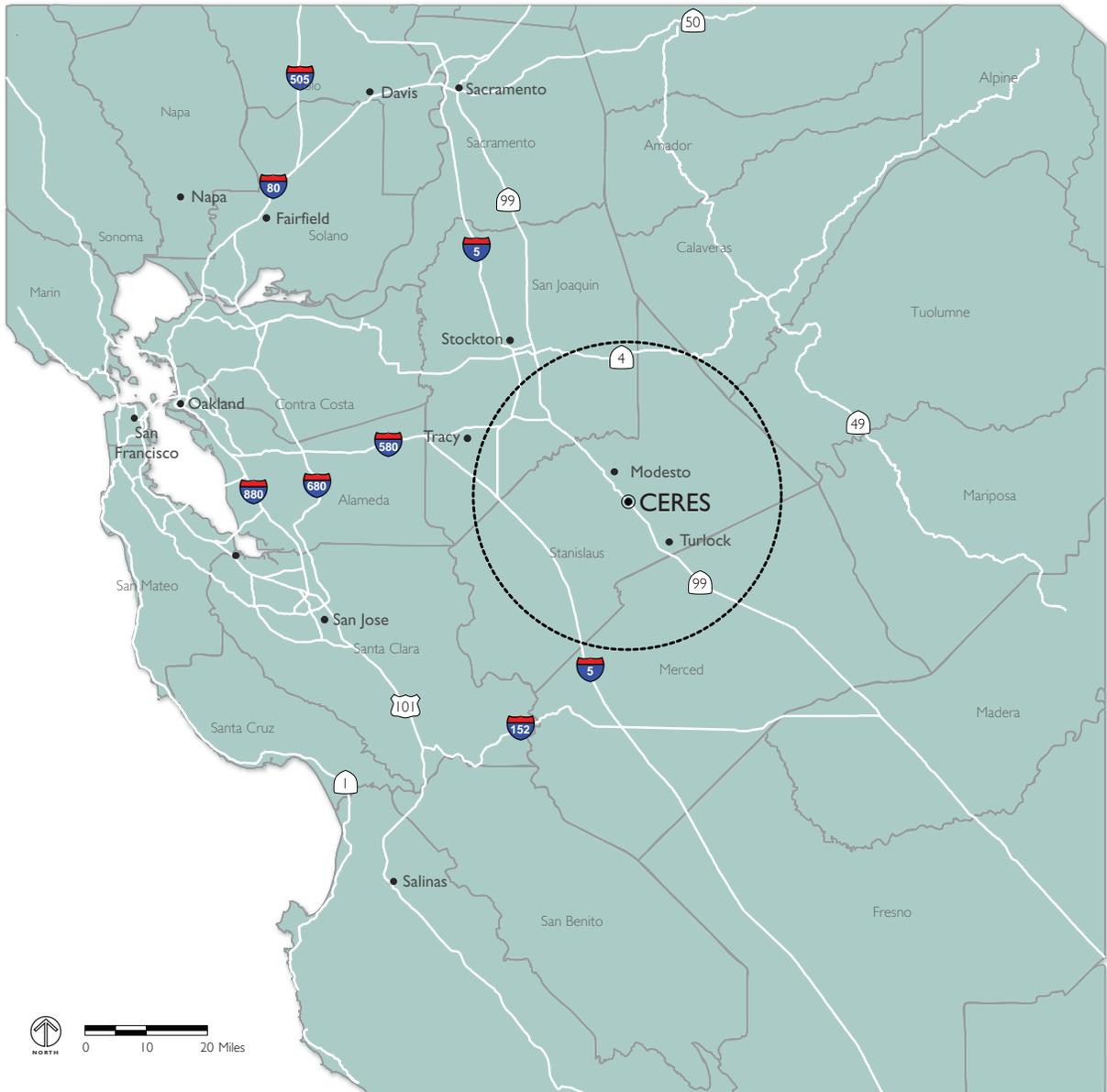
Throughout this document, the Specific Plan Area is also referred to as "Downtown." The Downtown Specific Plan builds off of the goals identified for Downtown in the General Plan to promote a rich mix of retail, residential, office, government, cultural and entertainment uses in Downtown. The Specific Plan also responds to policies in the General Plan focused on providing a pedestrian orientation, clear parking strategies, and physical improvements.

This chapter discusses the regional and local setting, purpose of and public process associated with the Downtown Specific Plan.

## A. Regional and Local Setting

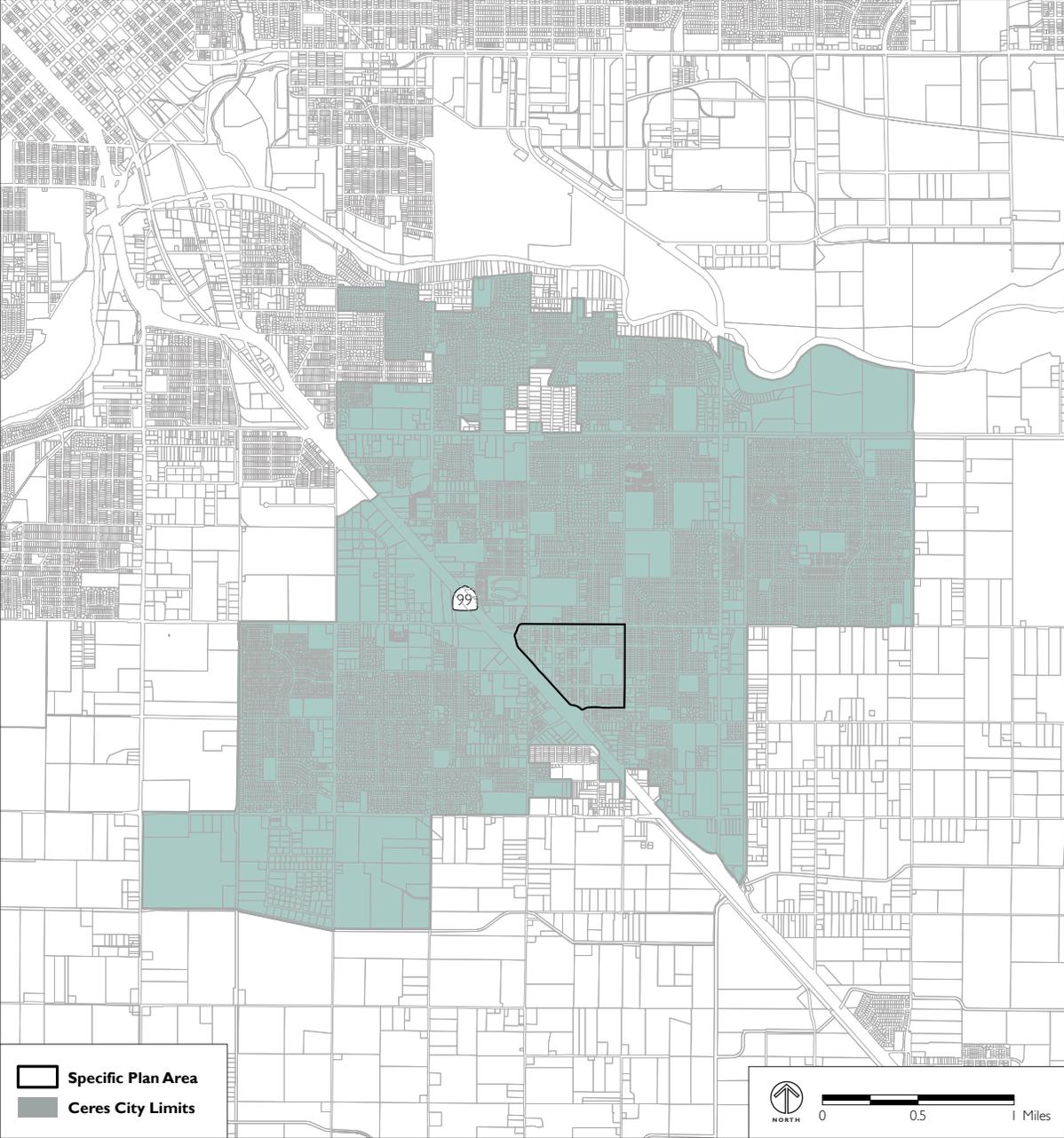
The City of Ceres is located in Stanislaus County, five miles south of Modesto and 13 miles north of Turlock. Ceres is bisected by State Route 99, the major north-south thoroughfare through the eastern side of California's Central Valley. Figure 1-1 shows the project site's regional location.

**FIGURE 1-1** Regional Location



The Specific Plan Area is located in the central portion of the city, just east of State Route 99. Figure 1-2 shows the local context of the Specific Plan Area.

**FIGURE 1-2** Local Context



## B. Specific Plan Area

The Specific Plan Area encompasses approximately 121 acres and includes all of Downtown. The Specific Plan Area is bounded by El Camino Avenue to the west, Whitmore Avenue to the north, Ninth Street to the east, and Park Street to the south. The Specific Plan Area is shown in Figure 1-3.

State Route 99 provides the major regional and local access route to Downtown. Access to State Route 99 is available from Whitmore Avenue, Second Street and Fourth Street within Downtown. Whitmore Avenue provides primary access to Downtown from the eastern areas of the city.

**FIGURE 1-3** Specific Plan Area



## **C. Purpose and Intent of the Plan**

This Specific Plan is intended to serve as the primary document and reference guide for future development in Downtown. This Specific Plan provides the community and decision-makers with clear documentation of the Downtown vision. Furthermore, it is intended to provide a clear policy and regulatory framework by which future development projects and public improvements will be reviewed. To augment these provisions of the Plan, guidance on design, potential public investments, and implementation strategies are provided. Potential opportunities for public-private partnerships, where the City would agree to work in concert with a developer in a mutually beneficial relationship to complete a project, are also identified.

## **D. Community Planning Process**

The Downtown Specific Plan was developed using an intensive public engagement process, which is described below.

### **Initial Steps**

The Downtown Specific Plan process began in late 2007. As an early outreach effort, a community survey was administered to Plan Area residents to gain an understanding of topics most important to them. Existing conditions in the Specific Plan Area were also examined as an initial step in the planning process. The findings of this process are summarized in Chapter Two of this document, and a more detailed review of existing conditions is provided in the Environmental Impact Report (EIR) conducted for this Specific Plan.

### **Stakeholder Interviews**

As a first step in working with the community, it was important to discuss the Downtown with those closest to it and with inside knowledge of the existing Downtown environment. To address this, the DC&E consultant team held discussions with several stakeholders to assess their perception of the Plan Area, identify important issues, and understand how stakeholders thought Downtown could be improved. Interviewees included staff members from various City departments, property owners, business owners, residents, school district personnel, and community service organization representatives. The information provided a foundation of understanding with which the planning process for Downtown could move forward, including providing insight that would inform the Community Workshops to follow.

### Community Workshops

The Community Workshop series began with a Visioning Workshop (Workshop #1) that was attended by approximately 35 people. Goals identified at this workshop included increasing visibility of Downtown, exploring new forms of housing, providing a new public gathering place, and improving Fourth Street conditions and business viability. Following the first workshop, City staff had a booth to publicize the Specific Plan process at the annual Street Faire.



Community Workshop #2.

At Workshop #2, participants were encouraged to engage in small group discussions, using maps to identify the location and nature of potential improvements in Downtown. Several ideas emerged from this workshop, such as a movie theatre, mixed use residential and retail development, gateway elements, and a desire for enhanced pedestrian links in Downtown.

At Workshop #3, participants were asked to review three land use alternatives and prioritize the ideas contained within them. The direction reinforced at this meeting was to ensure that Fourth Street remains the commercial heart of Downtown, that Downtown should be a safe and walkable environment, that a cinema use would be beneficial if it is feasible, and that streetscape improvements are important. Work



Community Workshop #3.

conducted at this workshop and previous workshops led directly to the development of a Preferred Alternative for Downtown's redevelopment.

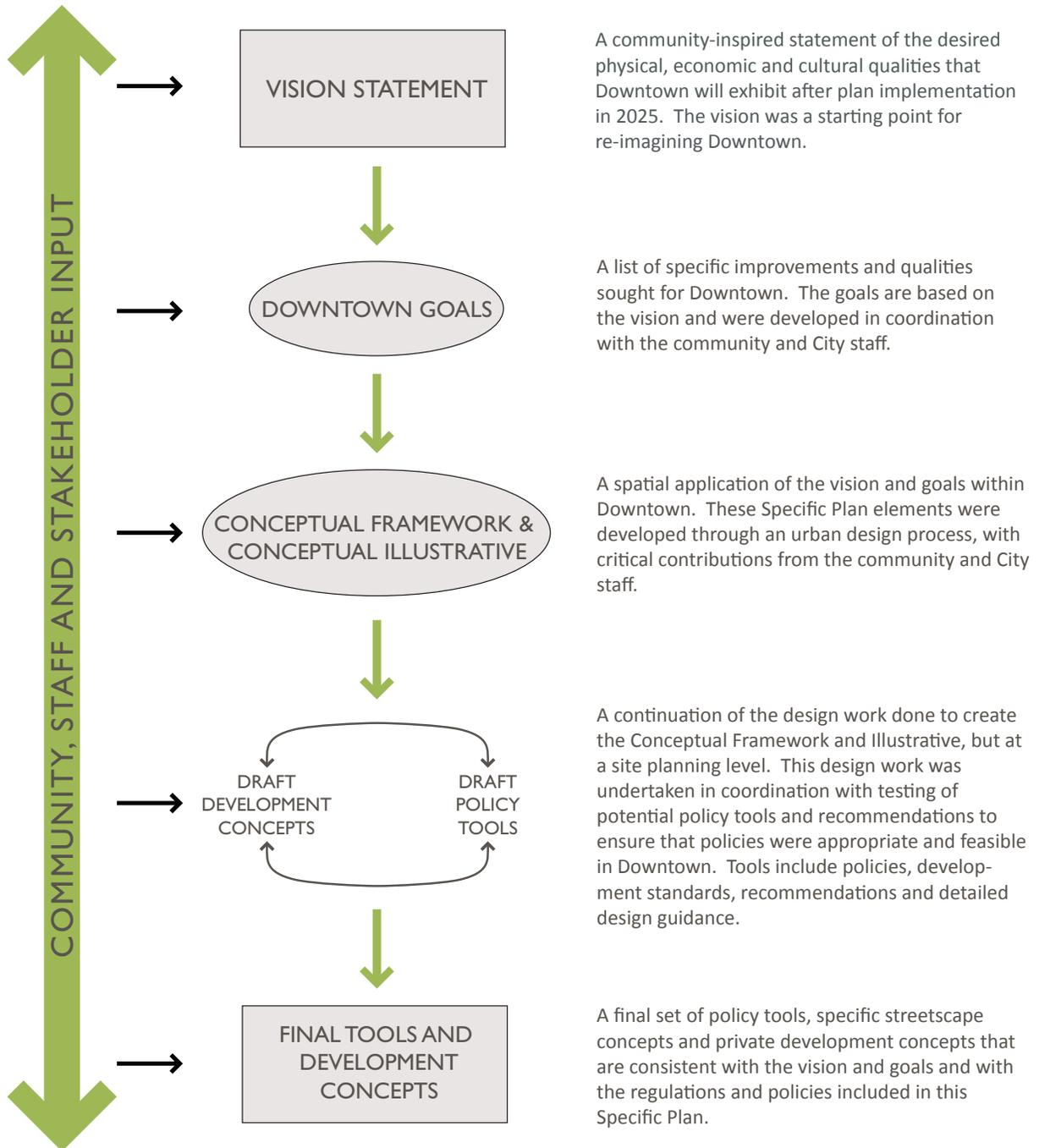
### **Staff Charrette**

In December 2008, staff members from all City departments participated in a charrette to synthesize the information from the previous workshops and work toward a preferred alternative. By working together, staff members were able to discuss the feasibility of ideas and to consider how community recommendations would impact the City from an array of perspectives. This important step served as a "reality check" and augmented the workshop series, which strongly influenced the development of the Preferred Alternative. The Preferred Alternative prescribes the preferred land use mix, design concepts and transportation improvements for Downtown. In December 2008, the Preferred Alternative was presented to City Council and attending members of the public.

### **Specific Plan Preparation**

With a Preferred Alternative in place, the consultant team developed this Downtown Specific Plan, which includes detailed guidance for achieving the Preferred Alternative in Downtown. The Preferred Alternative is discussed in more detail in Chapter Three, Vision, Goals and Concept. Figure 1-4 illustrates the process undertaken to develop this Specific Plan.

**FIGURE 1-4** Specific Plan Preparation Process



## **E. Statutory Requirements of the Specific Plan**

Under California law, Cities and Counties may complete Specific Plans to develop policies, programs, regulations and guidelines to implement the jurisdiction's adopted General Plan. A Specific Plan effectively establishes a link between implementing policies of the General Plan and the individual development proposals in a defined area.

### **Required Contents**

This Specific Plan has been prepared in accordance with the requirements of California Government Code Section 65451. As prescribed by law, the Plan includes text and diagrams that generally describe the following:

- The distribution, location and extent of all land uses, including open space.
- The proposed distribution, location and extent and intensity of major components of public infrastructure, such as transportation and drainage systems.
- The standards and criteria by which development will proceed.
- A program of implementation measures, such as financing measures, policies, regulations and public works projects.
- A statement of the relationship of the Specific Plan to the General Plan.

### **Findings of Consistency with the General Plan**

California law requires a Specific Plan to be consistent with a City's General Plan and that findings regarding consistency be included in the Specific Plan itself. Although the following amendments to the City's General Plan and Zoning Ordinance will be necessary to allow its implementation, the recommendations and objectives of the Downtown Specific Plan are consistent with the overarching goals of the Ceres General Plan, including the direction given for the Downtown area.

#### ***General Plan***

The General Plan must be amended alongside adoption of this Specific Plan. General Plan amendments will ensure that land use designations and their associated provisions are consistent between the Specific Plan and the Ceres General Plan.

#### ***Zoning Ordinance***

The Zoning Ordinance must be amended to implement the Specific Plan by creating a new Downtown Specific Plan Overlay Zone. This Downtown Specific Plan Overlay Zone will become a zoning classification within the City of Ceres Zoning Ordinance and will allow the development standards in the Downtown Specific Plan to augment or supersede the current zoning. If there is a discrepancy, the Downtown Specific Plan takes precedence.

## F. Plan Contents

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The additional chapters follow this introduction.

### ***Chapter Two: Existing Conditions***

This chapter provides an overview of existing conditions in Downtown.

### ***Chapter Three: Vision, Goals and Concept***

This chapter tells the “story” of the Specific Plan by providing a vision statement, plan goals, urban design concept, and a series of recommended improvements.

### ***Chapter Four: Development Concepts***

This chapter discusses potential public and private development concepts for Downtown. Detailed illustrations are included.

### ***Chapter Five: Policies***

This chapter provides the guiding policy statements intended for use by decision-makers and the public when considering new development or public investments in Downtown.

### ***Chapter Six: Land Use***

This chapter provides the land use framework and designations for Downtown, including the typical uses and densities allowed.

### ***Chapter Seven: Land Use Regulations and Development Standards***

This chapter provides land use regulations and development standards for new development in Downtown.

### ***Chapter Eight: Circulation***

This chapter discusses proposed circulation improvements in Downtown, including pedestrian, vehicular, bicycle and transit improvements.

### ***Chapter Nine: Public Facilities, Services and Infrastructure***

This chapter describes proposed improvements of public facilities to meet potential increased demand for public services and utility infrastructure.

### ***Chapter Ten: Implementation***

This chapter provides a strategy for implementing the Specific Plan over the next 15 years.

## EXISTING CONDITIONS 2

A key step in the Specific Plan process was a thorough assessment of existing conditions in Downtown. This chapter summarizes the results, beginning with a discussion of key strengths to build on as Downtown revitalizes.

## **A. Key Downtown Strengths**

Downtown has several existing strengths that will provide a strong foundation for the Downtown Specific Plan. These elements define and create the character of Downtown, and change in the Downtown should be geared at their preservation and enhancement.

### **Investment in Downtown**

The City has already begun investing in Downtown by recently constructing a Community Center on Third Street, which creates a new focal point for Downtown. The Ceres Community Center celebrated its grand opening on June 16, 2009. The Community Center provides assembly and conference space, a City Council dais, computer facilities, and houses the Parks and Recreation Department. It provides expanded numbers of employees into Downtown on a daily basis and generates pedestrian activity and market support for Downtown businesses. The Community Center is a Downtown draw and should catalyze future public and private investment in Downtown.



Ceres Community Center.

Fire station #1 was also recently constructed on Third Street in Downtown. Completed in 2003, it serves as the primary fire station in the City and as the administrative headquarters of the Ceres Emergency Services Division. Like the Community Center, this resource benefits Downtown by bringing employees Downtown on a daily basis,



Fire Station #1.

adding a unique and formidable presence through its architectural detail, and ensuring quick emergency response times to Downtown residents and businesses.

The Turlock Irrigation District (TID) recently chose Downtown as the location for one of its new offices. The new TID building, which opened its doors on November 20, 2009, is located on Third Street across from Whitmore Park. The TID building brings additional employees to Downtown and attracts visitors who will add to the market demand in Downtown.

### Fourth Street Commercial

Fourth Street, which has served as a central component of Ceres' economy and commercial heart since the city's beginnings, is currently home to several buildings that exhibit urban character appropriate for a downtown. Buildings along the southern blocks of Fourth Street are built to the sidewalk's edge and help frame the street. Most of these buildings are single story and a few are two stories. Buildings framing the Fourth Street/Lawrence Street intersection are examples of existing Downtown development that exhibit traditional "Downtown" character, including the Independent Order of Oddfellows (IOOF) building located at the southwest corner. These structures should be preserved as appropriate and considered as assets to build upon in the future.



Fourth Street Commercial.

### Mature Trees

Existing mature trees add a strong presence at certain locations in Downtown. Trees that are mature, healthy and non-invasive should be preserved where possible and when appropriate. Street trees benefit Downtown by providing shade to pedestrians and adding to visual character. Trees are most prominent along Fifth and Ninth Streets, and on segments of North, Fourth and Sixth Streets.



Trees on North Street.



Whitmore Home.

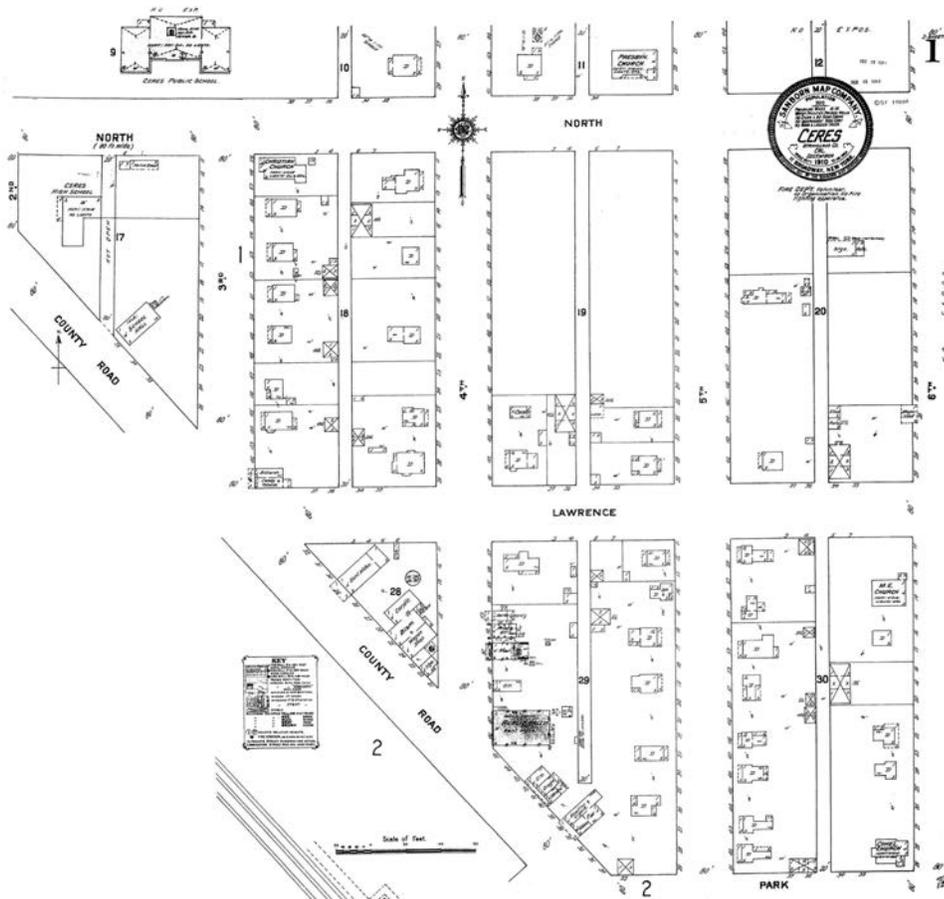
### Historic Resources

One of the treasures of Downtown is its historic resources. The first home in Ceres, the Daniel Whitmore Home, is located on Fifth Street in Downtown, along with the Whitmore Mansion and several other homes that were built around the turn of the twentieth century. These landmarks provide links to Ceres' past and add significantly to the character of Downtown. The Ceres Water Tower, located on Sixth Street, is a highly visible addition to the city's historic inventory. The Ceres Museum, located on Fifth Street, is not an historical structure, but provides a key resource for community members and visitors to learn about Ceres' beginnings and how the City has evolved over its history.

Just as Fifth Street in many ways represents the residential beginning of the City, Fourth Street represents the City's and Downtown's commercial beginnings. As shown on the next page on a Sanborn Map of Downtown from 1910, much of the commercial activity took place along the County Road and along the southern segments of Fourth Street, as it does today. In 1910, businesses on Fourth Street included a meat mar-

ket, printer, public hall and barber. The County Road has now expanded to become modern-day State Route 99 and pedestrian-scale development no longer fronts this corridor, but the commercial function of Fourth Street remains in tact. The southern portion of Fourth Street remains the heart of Downtown and home to many of its neighborhood-serving commercial businesses.

Downtown’s historic structures serve as both a reminder of its past and inspiration for its future. These structures can provide cues for future development and serve as cultural anchors within Downtown’s future urban fabric. Fourth Street’s history as the commercial heart of a small community can be preserved and now expanded to support the goals important to Ceres in the twenty-first century.



Ceres development in 1910.

### Existing Residents and Businesses

Much of the Downtown consists of single-family homes, particularly on the east side of Downtown. These homes create a strong residential base from which to build on and the residents who live there have an impact on the Downtown economy. Some homes have been converted to offices or retail, but the majority remain as housing. Downtown residents will continue to represent a key element of, and contributor to, Downtown for years to come.

Several businesses are located in Downtown and have a strong impact on the character of the area, as well as on its economy. They provide familiar restaurants, convenient services, add visual interest, and create activity in Downtown. While providing practical benefits, Downtown businesses also provide a venue for culture, community and interaction to flourish. Business owners can have a strong impact on Downtown's future and their dedication to its improvement will provide a vital asset to the implementation process.



Downtown single-family residential.



Whitmore Park.

### Alley System

The majority of Downtown's blocks are serviced by a system of alleys, which add to the overall street grid. The alleys contribute to circulation, provide easements for Downtown infrastructure, and will provide excellent opportunities for redevelopment in Downtown by offering additional circulation possibilities and providing potential access to new businesses and housing.

### Whitmore Park

Whitmore Park is the most notable and prominent public space in Downtown. It is a large, well-maintained park and should remain a key component of Downtown in the future. The block on which Whitmore Park sits was home to two schools in 1910, providing a location for a key community service.

Many years later, Whitmore Park still provides an invaluable community service as a park and is home to several mature trees and contains a variety of programmed spaces, including a gazebo, several lawn areas suitable for a variety of activities, and a veterans memorial. Whitmore Park has also hosted several special events in recent years, including farmers' markets, concerts and health fairs.

### **Access To and From State Route 99**

Downtown Ceres is unique among other downtowns in the Central Valley because it is clearly visible from State Route 99. This creates important opportunities to transform travelers on State Route 99 to new Downtown visitors. Highway access and visibility will also be attractive to developers, new business owners, business owners looking to relocate, and to people and families considering making Ceres their home. From an economic development perspective, access to and visibility from State Route 99 is a powerful draw for attracting potential investment to Downtown.

### **Proximity to Modesto City-County Airport**

Downtown should take advantage of its close proximity to the Modesto City-County Airport. Located just over four miles from Downtown, this airport provides excellent regional and national access to recreation destinations, as well as multiple business markets. The airport's ability to support the most common types of business aircraft creates an opportunity for Ceres to promote a business-ready Downtown. Studies have found that companies using business aircraft outperform non-users across every key financial and non-financial measure of business success.

## **B. Public Policy**

The Ceres General Plan contains several goals and policies that focus on Downtown or are city-wide goals and policies that apply to Downtown. Goals and policies that relate to Downtown are located in the following elements of the General Plan:

- Land Use and Community Design
- Transportation and Circulation
- Recreational and Cultural Resources
- Housing

The goals and policies in the General Plan speak to the uses, transportation options, services and housing types that are appropriate for Downtown Ceres and will promote a vibrant downtown.

### C. Community Character and Design

Community character in the Specific Plan Area is generally defined by the streets, urban context and connections, and buildings.



Residential street in Downtown.

#### Streets

The streets forming the perimeter of Downtown are generally more auto-oriented and the interior streets that form the downtown grid pattern are more pedestrian-oriented. Streets vary in configuration, but are generally defined by the presence of sidewalks and the orientation of on-street parking. Three traffic circles also contribute to the current character of streets in Downtown Ceres.

#### Built Form

Buildings and building orientation strongly impact the character of the Specific Plan Area. Buildings include public facilities, commercial buildings, and single- and multi-family housing units. Each building type creates a unique street condition, affects pedestrian experience and contributes to the overall physical character within the Specific Plan Area.



Typical development on southern blocks of Fourth Street.



Undeveloped area of the Whitmore Mansion grounds.

#### Opportunity Sites and Parking Areas

Downtown is also defined by opportunity sites, such as vacant or underutilized parcels and large parking areas. When located adjacent to the street, these sites often result in a discontinuous pattern of development, which is less pedestrian-friendly.

## D. Land Use

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Downtown Ceres contains a variety of land uses, most notably the following:

- Retail, both regional and neighborhood serving
- Office
- Public/Institutional  
(including City Hall, Police Headquarters, fire and several churches)
- Multi-family Residential
- Single-family Residential
- Parks
- Parking Areas
- Vacant Parcels

These uses are spread throughout Downtown, but pockets of similar uses are grouped in specific areas. Several public and institutional uses are clustered along Third Street. Small-scale commercial and retail uses are clustered on the southern portions of Fourth Street. Residential uses are generally located east of Fifth Street, north of Magnolia Street and west of Second Street.

## E. Demographics and Market Demand

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The Downtown Ceres Development Market Overview Report was submitted to the City in March 2008 during the initial stages of the planning process. The Report discusses demographic trends and projections, economic conditions and development potential in the Specific Plan Area. The key findings from this document are summarized below in this section.

### Demographic and Economic Findings

Since 2000, Ceres has experienced a rate of growth roughly double that of the State. By 2030, projections suggest that the Ceres population could increase by 30,000. This growth is driven largely by Ceres' close proximity to Modesto and Turlock. This future population growth will create potential opportunities for Downtown, particularly for housing and office uses.

### **Housing Market**

Like most places in the country, home values fell in Ceres in 2008, but decreases were less severe than in Stanislaus County as a whole. This suggests that as of 2008, the housing market in Ceres has remained relatively strong. The Terrace, a small subdivision of 28 small-lot single-family homes, has recently been developed near the Richard Way/Richland Avenue intersection. This project has been successful and additional phases are expected. Demand for multi-family homes has been higher than multi-family housing products (supply) in the past according to local real estate industry participants, and will likely stay strong upon economic recovery.

Townhomes are among the most economically feasible multi-family housing types for Downtown Ceres, since they have lower construction costs than more complex condominium projects. The Meridian, a townhome project at the Boothe Road/Whitehaven Avenue intersection, reinforces the market potential for townhomes in Downtown. The recent market success of a rental multi-family development on Tenth Street (between Roeding Road and Pyramid Avenue) near Downtown and comments from local real estate industry participants indicate strong future potential for rental properties in Downtown. Multi-family production, including both for-sale and rental units, is important for attracting the younger households, professionals, and other similar demographic groups most interested in living and working in a downtown environment and that would support expanded retail choices.

### **Retail Market**

Due to small lot sizes, small existing buildings, and the extensive and expanding retail corridor along nearby Mitchell Road, the market for neighborhood-serving retail is limited within Downtown. This suggests that retail uses with the greatest market potential in Downtown include small eateries, brew pubs, specialty food stores and other types of specialty retail that provide a different and unique tenant mix compared to the more typical suburban mix of national and regional retailers along Mitchell Road.

### **Office Market**

Demand for office space in Ceres is relatively low, particularly for larger employers. This is evidenced by low occupancy levels for office space along with low asking lease rates. Future demand for office space will likely increase as Ceres' population and office-based employment continues to grow, creating demand that could be met locally rather than in nearby existing office centers such as Modesto. There may also be opportunities for new types of office products, such as office condominiums targeted at smaller firms wishing to own their own space.

## F. Transportation and Circulation

This section discusses the existing vehicular, pedestrian, bicycle and transit conditions within the Specific Plan Area. This section summarizes the key findings from the Existing Transportation Conditions Memorandum submitted to the City in March 2008.

### Vehicular Circulation

Traffic in Downtown currently operates at an acceptable level under the level-of-service (LOS) standards described in Ceres General Plan. This indicates that opportunities exist to intensify uses and accommodate associated increases in traffic.



Whitmore Avenue.



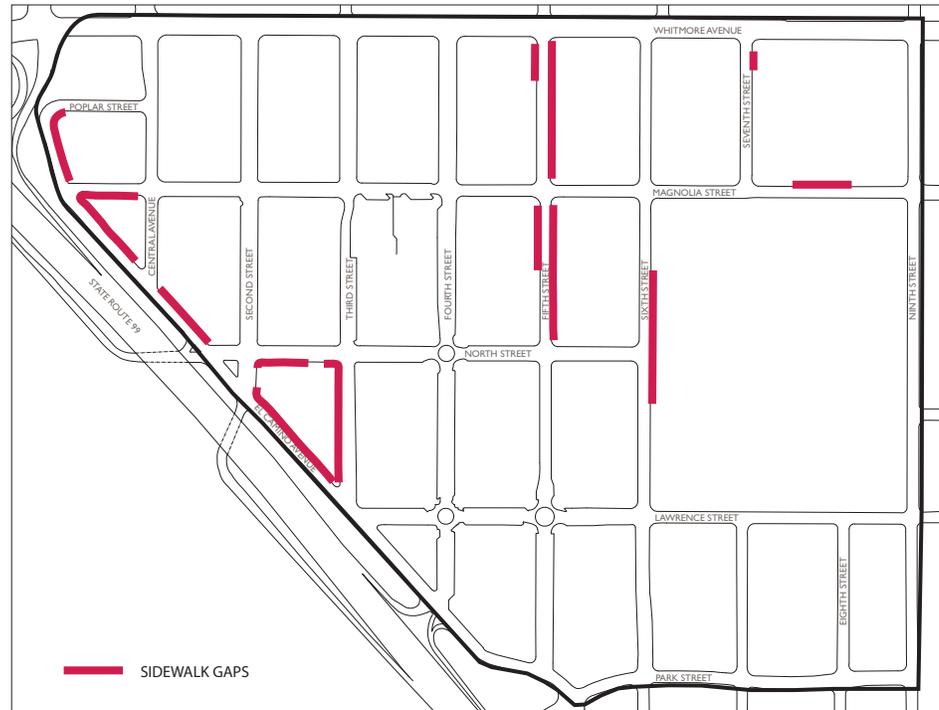
Crosswalk at Whitmore Avenue/Third Street intersection.

### Pedestrian Circulation

Sidewalks are generally present throughout Downtown, with very few gaps. Gaps in the sidewalk system are shown in Figure 2-1. Downtown sidewalks are generally between four and five feet wide with the exception of the two southern blocks of Fourth Street, where they are considerably wider.

Crosswalks are provided at traffic signals along East Whitmore Avenue in Downtown, as well as at some of the unsignalized intersections along this corridor. Traffic signals are also equipped with pedestrian crossing signals that let the pedestrian know when it is safe to cross. Crosswalks are also prominent at intersections near Walter White Elementary School. Crosswalks are also provided at the Park Street/El Camino Avenue intersection and across Fourth Street at the El Camino Avenue/Fourth Street/State Route 99 northbound ramps intersection. Field observations indicate that pedestrian activity is highest near schools in Downtown, particularly at arrival and dismissal times.

**FIGURE 2-1 Sidewalk Gaps**



**Bicycle Facilities**

Bicycle facilities are typically classified into three groups:

- **Bike paths (Class I)** – Paved trails that are separated from roadways.
- **Bike lanes (Class II)** – Lanes on roadways designated for use by bicycles through striping, pavement legends and signs.
- **Bike routes (Class III)** – Designated roadways for bicycle use by signs only; may or may not include additional pavement width for cyclists.

The Specific Plan Area contains few bicycle facilities. No Class I off-street bike paths or Class II on-street marked bike lanes exist. Magnolia Street is designated as a Class III bike route.

**Transit**

Both local and regional transit service is provided in Downtown. Ceres Area Transit (CAT) and Stanislaus Regional Transit (StART) provide fixed route service within the Downtown.

Local fixed route transit service in Ceres is provided by the City of Ceres. Three CAT routes serve Downtown. Routes A and B operate during weekdays. Route A serves the eastern portion of the City, while Route B serves the western portion of the City. These routes overlap in Downtown. Service is provided on one-hour headways. Weekend service is provided by Route C, which consolidates portions of Routes A and B and operates primarily on Magnolia Street within Downtown.

StART Route 15 serves Downtown by connecting Ceres to Modesto, Keyes and Turlock. Bus stops in Downtown are located on Magnolia Street near City Hall and near Ninth Street. The route operates eight round trips per day with buses arriving approximately every two hours. Weekend service consists of six round trips per day with similar headways to weekday service. Hours of service on weekends are more abbreviated.



Transit stop at City Hall.

## G. Parking

As part of the Specific Plan process, a Downtown Ceres Parking Analysis was prepared by Fehr & Peers. This section summarizes the findings of the report, which was submitted to the City in February 2009.

Downtown parking spaces were inventoried and include 1,521 on-street parking spaces and an additional 263 off-street parking spaces. These figures do not include all surface parking areas within Downtown. Some were excluded for the following reasons:

- Minimal use occurs on weekdays, such as the First Baptist Church
- Parking areas were restricted, such as City Hall
- Parking areas were only accessible by private driveways

The parking assessment indicates that parking is generally available within Downtown and accommodates demand throughout the day. The highest parking utilization rate for Downtown for any given hour during the day was 29 percent. This utilization rate decreased to below 20 percent after 5 p.m.

However, certain areas in Downtown had less parking available because of their proximity to commercial uses, civic uses or employment centers. For example, less parking is available near City Hall and other civic buildings because these buildings receive visitors throughout the day. It should also be noted that parking availability in these



Underutilized parking on Fourth Street.



Occupied parking on Fourth Street.

areas does increase significantly during evening hours. The following areas experience below-average parking availability during daytime hours:

- Areas around the library, school and School District offices.
- On-street parking along Second Street near City Hall.
- Surface and on-street parking near the southern portion of Fourth Street also experienced above-average parking demand.

Lower parking availability in these areas is attributable to the number of nearby retail stores and restaurants that have only minimal off-street parking supply. It should be noted that while parking availability was lower in these areas than in others (reaching 85 percent utilization or higher for a small number of select blocks), additional parking was available within one block.

## H. Infrastructure

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As part of the Specific Plan process, an Existing Conditions Infrastructure Summary was prepared by Jacobs Engineering. This report was submitted to the City in March 2008. This section summarizes the report by presenting the key findings related to existing infrastructure conditions in Downtown.

### Stormwater

Storm drainage in Downtown flows to two pump stations. Drainage flows north to the pump station on El Camino at Whitmore and south along the Pine Street overcrossing to an additional pump station southwest of State Route 99. The two pump stations have enough capacity to serve existing Downtown uses, although some of the connecting pipes are undersized. There is also a system of dry wells, although some are currently not functioning efficiently. Inefficiencies have resulted from improper well maintenance and allowing gravel fines to enter the rock, reducing capacity. Existing pipes are also rusting.

### Sewer System

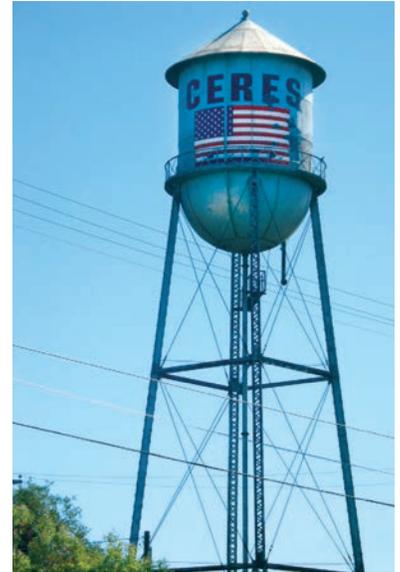
Sewage from Downtown flows toward the Pine Street Lift Station at the northwest corner of Pine Street and Railroad Avenue. Existing sewer infrastructure in Downtown ranges from 6-inch to 8-inch pipes. The Wastewater Treatment Plant, located at East Service Road and Morgan Road, currently treats approximately 3.1 million gallons of wastewater per day. The Plant has a capacity to treat 6.3 million gallons of wastewater per day, which is adequate to serve current development in Ceres.

**Water**

According to modeling efforts conducted by Jacobs Engineering and flow information provided by the Ceres Fire Department, the water system has sufficient capacity to serve the existing development in an average daily and maximum daily demand scenarios, as well as under fire flow conditions. However, the water system is not currently equipped to provide adequate pressure to accommodate a maximum daily demand scenario and still maintain adequate fire flow capabilities.

**Dry Utilities**

Electrical service is currently provided by the Turlock Irrigation District. Electrical service in Downtown is provided via overhead electrical facilities. Adequate electrical service is available in the downtown area, both for existing and potential new development. Pacific Gas & Electric (PG&E) provides natural gas to Downtown. PG&E has indicated that there is sufficient capacity to serve existing development, and that there is likely capacity to serve potential new development. AT&T provides telecommunications services to Downtown, including telephone and DSL internet service. Additionally, AT&T operates a switching station on Third Street within Downtown. Cable television and cable internet service is provided by Charter Communications. Charter expects that provision of cable services for future development will be feasible.



Ceres Water Tower.

**I. Cultural Resources**

Cultural resources include both archaeological and historical resources. There is no evidence of prehistoric or buried historic cultural resources within Downtown. A review of architectural records and a partial architectural survey was conducted for Downtown and indicates that the Daniel Whitmore Home is the only property listed on the National Register of Historic Places. The Whitmore Mansion is another notable cultural resource not listed on the National Register. Additionally, several other residential properties built around the turn of the twentieth century and the Ceres Water Tower were identified as cultural resources during the planning process, but do not yet warrant inclusion on the National Register of Historic Places.

**J. Community Services**

This section characterizes existing services for police, emergency services, schools, the library, parks and solid waste disposal.

### Police

The City’s Police Division currently consists of 46 sworn officers, 22 non-sworn personnel, and 45 volunteers. The number of officers on patrol at any one time ranges from three to eight officers, plus a shift supervisor. The average staffing is one officer per patrol unit. Response times are categorized according to the severity of the offense or complaint. Depending on the priority of the offense, the department estimates it can have an officer on the scene in approximately six minutes.



Ceres Police Building.

### Emergency Services

Fire Station #1 is located Downtown at the corner of Third and North Streets, and houses on-duty fire fighters and administrative staff. The Emergency Services Division is staffed by 34 uniformed personnel, including the Chief, and one non-uniformed personnel member. At any one time, ten fire fighters comprise a duty shift. Average response time varies depending on the circumstances and type of emergency. The Division estimates that first response is usually between three and five minutes with additional backup arriving as needed.

### Solid Waste Disposal

Solid waste disposal in Downtown is provided by a franchise agreement with Bertolotti-Ceres Disposal. Solid waste and curbside recycling collection is provided for both commercial and residential uses.

## Schools

The following schools serve students living in Downtown. The following information was provided by the Ceres Unified School District.

### ***Walter White Elementary***

Located Downtown, Walter White Elementary had an enrollment of approximately 570 students for the 2008-2009 school year. Walter White Elementary is currently at capacity.



Walter White Elementary.

### ***Blaker-Kinser Junior High***

Located outside the Specific Plan Area, Blaker-Kinser is fed by Walter White Elementary and several other elementary schools. Blaker-Kinser had approximately 840 students for the 2008-2009 school year and is currently over capacity.

### ***Central Valley High School***

Located just outside Downtown, Ceres High School is fed by Blaker-Kinser, Walter White Elementary, and other schools within the District. Central Valley High is currently under capacity and had an enrollment of approximately 1,530 students for the 2008-2009 school year.

### ***Argus Continuation/Endeavor Alternative High School***

Located Downtown, Argus Continuation had an enrollment of approximately 300 students for the 2007-2008 school year. Argus and Endeavor is currently at capacity.

## Library and Community Center

This section discusses the existing Downtown library and the Ceres Community Center:

### *Gondring Memorial Library*

The Gondring Memorial Library, a branch of the Stanislaus County library, is located Downtown. The Stanislaus County system has a main library and 13 branch libraries. The Gondring Library currently has three full-time employees and four part-time employees. The library is approximately 4,500 square feet and contains approximately 38,000 volumes.



Gondring Memorial Library.

### *Ceres Community Center*

The Community Center provides recreation, cultural and educational opportunities and features a large assembly room with a capacity of 315 people, a small assembly room with capacity of 127 people, a large kitchen, a recreation fitness room, a computer lab, a senior citizen room, a teen room, an arts and crafts room, a City Council dais, recreation staff offices and several meeting rooms.



Whitmore Park.

## Parks

Whitmore Park is the only significant open green space within Downtown. The park is located at the corner of Third and North Streets and is approximately 1.5 acres in area. Features include a gazebo, picnic area, dance slab, barbeque pits, a rose garden, and a war memorial. The park is regularly used throughout the year for events, such as farmers' markets, health fairs and other community events.

## VISION, GOALS & CONCEPT 3

As we discussed in Chapter One, a thorough community visioning and design process was undertaken to inform the development of this Specific Plan. The result of the public process is captured in the Vision, Goals and Framework contained in this chapter. The vision is expressed in the Downtown Vision Statement on the next page. The following section sets out seven clear goals for achieving the vision. It also provides sound strategies to accomplish those goals in a way that is appropriate for and beneficial to Ceres. Applying this vision and these goals to the actual streets, parcels and buildings in Downtown led to the creation of a Framework for guiding physical improvements and new development in Downtown.

The driving principle behind this effort is that the Downtown must be designed in a visionary, yet realistic, way that reflects the desires of the community. This chapter describes the end result that the Specific Plan intends to achieve, but the development and public improvements will be strategically phased during implementation. For a more detailed discussion of phasing, see Chapter Ten, Implementation.

## **A. Downtown Vision Statement**

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This vision statement, crafted with the collaboration of the community and stakeholders, describes the physical, economic and cultural qualities that Downtown will exhibit after this Specific Plan is implemented.

Downtown will once again become the vibrant heart of the City of Ceres, as this area was during Ceres' beginnings. It will be a key shopping and entertainment destination, an employment center, and a government center for Ceres residents and out-of-town visitors alike. Downtown will be an established and unique neighborhood as evidenced by its quality architecture, the preservation and celebration of historic landmarks and structures, and attractive public spaces and plazas. Downtown will be the go-to location for local residents wishing to take in a movie, entertain visitors, host an event or socialize with their neighbors. Downtown will be a safe and active environment that celebrates Ceres' historic past and diverse culture, and provides community spaces and streetscapes unlike any others in the city.

Downtown will be a primary option for existing and prospective residents looking for housing in Ceres. A wide variety of rental and ownership housing opportunities of different types and sizes will be available. Housing will be affordable to Ceres residents of all income levels, providing options for all ages, from young singles to senior citizens. Families will consider Downtown an ideal place to make their homes and raise their children.

Downtown will be a local and regional model for sustainability. Infill development will reduce the need to develop on greenfield sites elsewhere. Downtown will provide for alternatives to driving by prioritizing pedestrians in design and by achieving densities that will support transit. It will include green building technologies in new development and green stormwater mechanisms in new infrastructure. It will provide a livable neighborhood for members of all income groups. Downtown will be economically sustainable as well, providing jobs for local residents and desirable locations for businesses and merchants. Downtown will be vital to Ceres' economy.

## **B. Downtown Goals**

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This section outlines seven realistic goals, which will result in achievement of the vision for Downtown. Within the goals, key strategies to accomplish the goals in Downtown Ceres are described. The strategies clarify how the goals are unique to Ceres and how they can feasibly be accomplished Downtown. The goals in this section evolved from

the collaborative community planning process undertaken for the Specific Plan, including input from community members, City staff and the project consultant.

### **Goal 1: A Sustainable Downtown**

Making Ceres more sustainable was a desire expressed by participants during the planning process. For Downtown, sustainability means a future development pattern that minimizes impact on the natural environment, spares land at the periphery of the city, champions conservation of water and other resources, and supports walking, bicycling and transit. Compact mixed use development in Downtown supports the goal of sustainability by preserving undeveloped land at the outskirts of the City. Higher-density development also supports walking, biking and transit. By making sustainable transportation alternatives viable, Ceres will begin addressing long-term regional air quality and global climate change concerns.

With implementation of this Specific Plan, infrastructure will be rebuilt to function efficiently and support a new, compact Downtown in Ceres. Alternative stormwater infrastructure technologies will assist in achieving sustainability goals by reducing stress on the existing system and preventing some stormwater runoff from entering regional watersheds. Future Downtown development will also contribute by including water conservation measures. By incorporating smart infrastructure systems, Downtown will serve as a model for other parts of the City, region and state.

Economic sustainability will also be a cornerstone of Downtown Ceres in the future. The Specific Plan will lead to new business investments in the Downtown, which will result in an influx of jobs for local residents. A range of housing in Downtown can also provide opportunities for young families and those looking for homeownership opportunities in Ceres, and will also assist Ceres in addressing its fair share of regional affordable housing needs.

### **Goal 2: An Economically Viable Downtown**

It is critical that economic conditions be improved in Downtown, where the City and its commerce originally began and where there is strong interest in its preservation and expansion. The primary strategy for achieving an economically viable Downtown Ceres is ensuring a rich mix of uses that collectively support one another. In Ceres, the general use mix is already present, but can be enhanced and expanded. New and diverse building types in Downtown can accommodate a wide variety of tenants, which will help ensure Downtown Ceres will remain a key center for commerce and will remain that way in the long-term.



Whitmore Park.



Fourth Street storefront.

Existing businesses will be retained and expanded, and new complementary businesses will be established. New high density housing, a movie theater, new office development and expanded civic uses will all contribute to the growth of the Downtown economy by providing spending power and spill-over shopping, particularly from large employment or entertainment centers. An improved Downtown environment will encourage developers interested in Ceres to look to the Downtown to invest. With implementation of this Downtown Specific Plan, Downtown will emerge as a regional destination, with an increase in visitors and new businesses.

An economically viable Downtown will lead to new jobs for Ceres residents, and it will boost the local economy by bringing in outside spending from increased regional visits to Downtown. Furthermore, new development that supports a rich Downtown economy will result in increased tax revenue for the City that can be re-invested in the Downtown and other citywide priorities.



Historic Ceres Water Tower.

### **Goal 3: An Historic Downtown**

Downtown marks the starting point of Ceres. Even prior to incorporation in 1918, Ceres was already a village in its own right, and everything in Ceres was located where Downtown is today. Ceres served as a small residential and commercial community and in those days was bounded roughly by North Street, Sixth Street, Park Street and what has become State Route 99. This Specific Plan seeks to restore the notion of Downtown as Ceres' true center and enhance the role of landmarks and historic structures by integrating them with public improvements.

Downtown historic resources, such as the Whitmore Home and Whitmore Mansion, will not only remain integral components of the Downtown but also serve as centerpieces for its future. Historic resources can be more appropriately celebrated and also serve as catalysts for future development. The resulting Downtown will be an attractive mix of development that shows Ceres' dedication to its history. New development will respect and accentuate these landmarks, resulting in a Downtown environment that has a special mix of old and new. The principle of preservation must remain at the forefront of redevelopment as new development and improvements are interwoven into the existing urban fabric.

### **Goal 4: A Diverse and Unique Downtown**

Downtown must build on existing attributes to enhance Downtown's character, in a way that is appropriate for Ceres. Downtown's character will become evident through the integration of existing elements, such as the Whitmore Mansion and the Ceres

Water Tower, with new design and development. Downtown character will also develop through the design and provision of unique streets, public gathering spaces, parks, and through public events that are held there. With implementation of this Specific Plan, the diverse and unique qualities of Downtown will also become evident to passers-by on State Route 99, drawing them in for a closer look.

### Goal 5: A Celebrated and Vibrant Downtown

People are critical to creating a successful Downtown, and they contribute strongly to their character. The presence of people and the activity they create must be accommodated as Downtown redevelops. Downtown will build on the existing tradition of special events like the street faire and concerts in Whitmore Park, becoming a location for additional events.



Residential development.



Ceres Community Center.



Pedestrians activate spaces.

Downtown will become a 24-hour neighborhood with housing mixed with daytime businesses, such as offices, civic employment centers and commercial services. Nighttime uses will also contribute to Downtown vibrancy with new housing, restaurants, a cinema and nighttime events. To further support the creation of an active environment, there will be a system of public gathering spaces, including new plazas, enhanced park space, pedestrian paseos and new pedestrian-oriented streets. These new elements will support special events, and they will create a higher quality of life for new residents and employees, visitors, students, families and seniors. Downtown will become a place to “hang out,” not a single-stop destination.

### Goal 6: A Multi-modal Downtown

Providing for transportation choice is a priority for Downtown. Ceres must prioritize pedestrians, bicycles and transit “modes” in Downtown. However, many residents and visitors will continue to drive, so Downtown should develop a circulation system that accommodates traffic and parking, but does so in coordination with the other modes. A system of parking can provide drivers with a clear understanding of where

### 3 VISION, GOALS & CONCEPT



Transit stop.

they can find parking when visiting Downtown businesses. On-street parking should be preserved to allow for short turnover trips. As Downtown redevelops and on-street parking becomes less readily available, additional public and private surface parking opportunities will be critical in managing parking.

Parts of Downtown already exhibit walkable qualities, including the southern blocks of Fourth Street, which have provided a pedestrian-friendly environment for decades in Ceres. These elements can be expanded and enhanced to redefine the entire Downtown as a walkable neighborhood. By investing in pedestrian-oriented streetscape improvements in coordination with private development, Downtown will become a neighborhood that not only supports walking, but makes it comfortable and enjoyable.

Bicycle facilities are a critical component of multi-modal downtowns. Ceres has already begun establishing this component by signing and designating Magnolia Street in Downtown as a bike route. Bike facilities will be expanded during implementation of the Specific Plan to ensure that biking is an attractive and feasible option for Downtown residents. Private development will complement Ceres' investments by providing bicycle parking.

In the long term transit will become a more viable option for Downtown Ceres. Currently, the low-density character of Ceres results in infrequent transit opportunities, particularly for regional travel. As higher-density development occurs in Downtown, transit opportunities will expand.



Downtown theater in Alameda.

#### **Goal 7: A Safe Downtown**

Public safety must be maintained during the growth and expansion of Downtown. Families should feel comfortable raising their families in Downtown. Students from Downtown and nearby schools should be able to safely visit Downtown, meet with friends and hang out in its public spaces. Downtown should be a safe place during the day and at night.

New compact mixed use development will contribute to accomplishing this goal by providing “eyes on the street.” The activity and presence of people in mixed use and downtown settings during the day and night can often be a deterrent to crime. Ceres can also address safety in Downtown by providing adequate lighting and by ensuring that new private developments incorporate “Crime Prevention Through Environmental Design” (CPTED) principles. A safe Downtown and a perception of safety will benefit Ceres by inviting potential residents and other prospective investors.

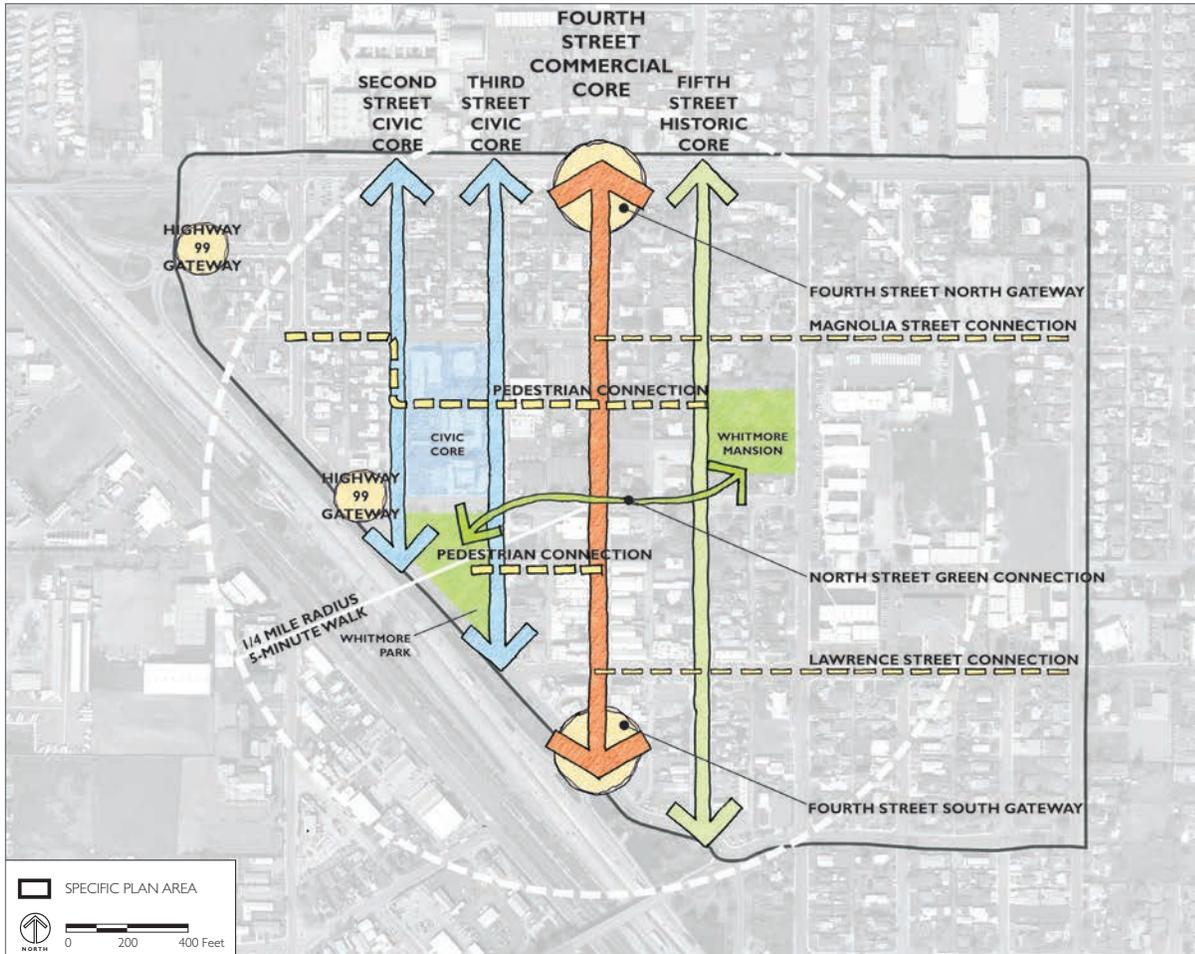
### C. Urban Design Framework

This section shows how the vision and goals can be physically realized in Downtown Ceres. Figure 3-1 is a Framework diagram that will inform future improvements and new development. Following the Framework discussion is an Illustrative Plan providing a detailed look at how the concepts in the Framework may be achieved.

#### Distinctive North-South Streets

A strong and identifiable character will be developed for several of the north-south streets in Downtown. For these streets, a unique character is already in place but needs to be strengthened. The character envisioned for these streets is described below.

**FIGURE 3-1** Downtown Conceptual Framework



**Fourth Street Commercial Core**

Fourth Street should serve as Ceres’ “main street,” functioning as a major activity center and the commercial heart of Downtown.

**Second/Third Street Civic Core**

Second and Third Street currently contain most of the civic functions of Downtown. The emerging character along these streets should be reinforced by promoting this area as Downtown’s government and office core.

**Fifth Street Historic**

Fifth Street is home to the Whitmore Home, Ceres Museum and the Whitmore Mansion, and the historic Ceres Water Tower is nearby on Sixth Street. Fifth Street should serve as an historic corridor for Ceres, spanning from the existing traffic circle at Lawrence Street to Whitmore Avenue at the north.

**Green Ceres**

Green Ceres is a concept developed for Downtown that focuses on ensuring sustainability and environmentally sensitive development and public improvements. Green Ceres also calls for increased access to green spaces and trees in Downtown.

**North Street Green Connection**

A new green space complementing the existing Whitmore Park is proposed at the Whitmore Mansion on Fifth Street. North Street will become a “green connection” linking this new Whitmore Green to the existing Whitmore Park, as shown in the diagram below. The street will be lushly planted, provide generous setbacks where possible, and incorporate green stormwater management systems.



**Green Infrastructure**

The Green Ceres concept also calls for sustainable improvements to public spaces, such as street rights-of-way. Through careful design and engineering, measures can be incorporated into street rights-of-way to help manage stormwater from streets, thereby reducing strain on the traditional stormwater infrastructure and helping to prevent pollutants that would otherwise flow into watersheds.



Bioswale within sidewalk.

**East-West Connections**

To complement the North-South streets concept discussed above and reinforce the Green Ceres concept, new pedestrian and bicycle connections running east-west should link the streets together, connect important destinations and lead to Fourth Street.

**Pedestrian Paseos**

East-west connections should be established through a series of paseo networks. These are pedestrian-only walkways that will cross midway through the long blocks flanking Fourth Street downtown. These will connect the Civic Core across Fourth Street to the Whitmore Mansion, and Whitmore Park to some of Ceres' most historic resources, including the Whitmore Home and Ceres Water Tower.

**Gateways and Wayfinding**

Gateways and wayfinding signage will help define Downtown to the visitor, provide a "branding" opportunity and make it easier to get around. Gateways and symbols that help establish Downtown as a destination within Ceres and signify that it is undergoing change will be crucial to the success of its revitalization.

**Gateway Opportunities**

Opportunities for enhanced gateways into Downtown include northern and southern entries to Fourth Street, the intersection of Second and North Streets, and the new off-ramp leading northbound SR 99 travelers to Whitmore Avenue. Gateways at these locations should include signage and special landscaping treatments. The north and south entries to Downtown on Fourth Street shall be further enhanced by thoughtfully designed gateway buildings that provide architectural character at these important entries to Downtown. These north and south entries will be critical to the success of Fourth Street, which as the heart of Downtown will reverberate through the rest of the area.



Gateway signage.

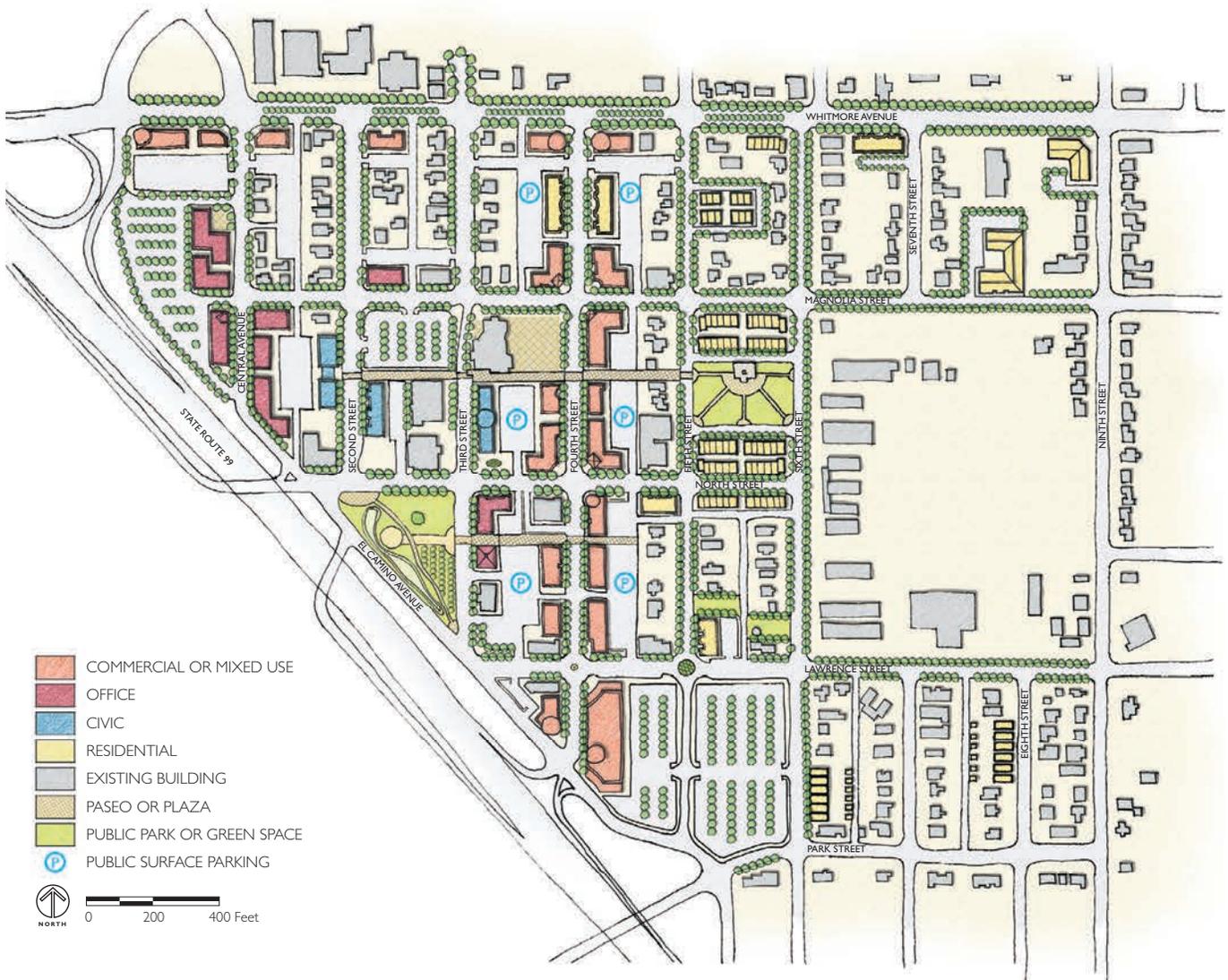
***Wayfinding***

A wayfinding system will be established to provide vehicular traffic with increased awareness of Downtown and reinforce the character of the most important streets. Wayfinding signage will be established on Whitmore Avenue to identify the Civic Core, the Fourth Street Commercial Core and Fifth Street Historic District. Streetscape improvements along Whitmore Avenue in Downtown will also help improve the image of Downtown to travelers on this busy street.

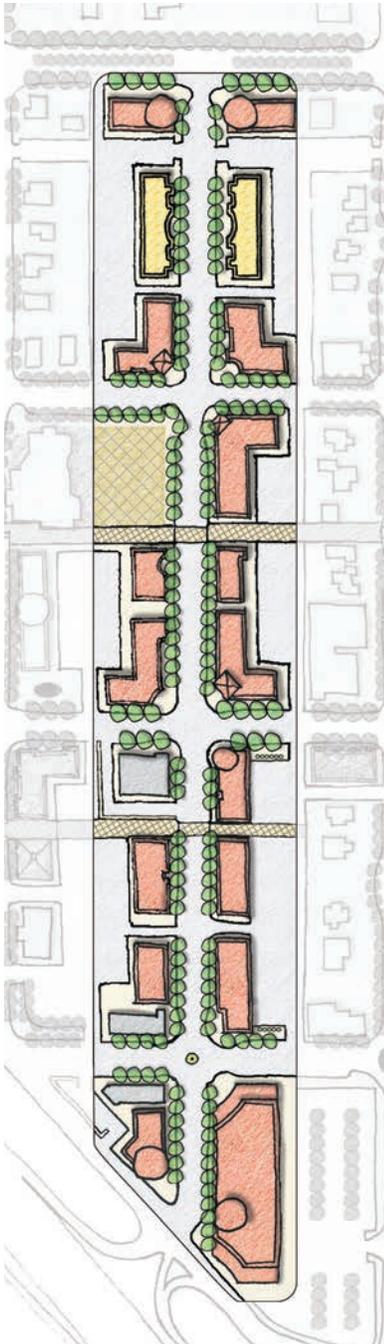
### D. Conceptual Illustrative Plan

This section is intended to illustrate how the big-picture ideas in the Framework diagram could be achieved in Downtown. Figure 3-2 shows a Conceptual Illustrative Plan that illustrates the potential buildout of private development and public improvement opportunities recommended by this Specific Plan. The Illustrative Plan is designed to-scale to ensure feasibility, but it is strictly conceptual. The exact locations of new development and improvements will be determined during implementation of this Specific Plan, in more detailed design processes and during approval of specific development projects. The following development projects are encouraged to achieve the vision and accomplish the goals for Downtown.

**FIGURE 3-2** Conceptual Illustrative



**FOURTH STREET MIXED USE**



**Fourth Street Mixed Use Development**

New mixed use development is the prominent use envisioned for buildings that front onto Fourth Street. This is particularly relevant at the north and south entries to Fourth Street, where buildings should both support the vision for mixed use in Downtown and serve an additional role of demarcating entry to the heart of Downtown. Buildings will be developed at a variety of scales, primarily with ground floor retail and upper floor residential. Some of the existing building stock is also expected to remain. Upper floor uses should be flexible. Recommended upper floor uses include townhomes, apartments and potentially office. Private development along Fourth Street may also create opportunities for outdoor seating, publicly accessible plazas, and additional landscape features, particularly in areas that are adjacent to pedestrian areas within the street right-of-way. To complement new Fourth Street development, a new civic plaza is proposed where the current Community Center parking area is located. This parking will be converted in later phases of implementation, as Ceres' civic areas are expanded and the northern blocks of Fourth Street are redeveloping. Chapter Four, Development Concepts, shows a more detailed conceptual proposal for new development along Fourth Street.

Fourth Street is the most appropriate street in Downtown on which to focus this development type, particularly since it was the original commercial activity center in Ceres and many of these qualities remain intact. The redevelopment of Fourth Street should also capitalize on excellent access to and from Downtown provided via State Route 99. This type of development will have a strong influence on the character of Fourth Street, but the energy they create will also spread to the surrounding areas of Downtown.

### Fourth Street Cinema

A potential Downtown Cineplex is envisioned for the Plan Area. The Cineplex should relate to Fourth Street and enliven the Downtown. One potential location for this Cineplex is on the two blocks currently bounded by Fourth Street, El Camino Avenue, Park Street, Sixth Street and Lawrence Street.

A cinema is particularly suitable at this location due to the close proximity of State Route 99. The emissions from traffic on this highway make residential and mixed use development containing residential uses infeasible, making an indoor entertainment use highly appropriate.

A new cinema on Fourth Street will provide an economic anchor for Downtown, generate increased pedestrian activity at night, and provide a desired entertainment venue for residents. Downtown businesses will benefit from spillover spending from local moviegoers and those who come from nearby cities.



### Fourth Street South Gateway

A triangular block at the southern foot of Fourth Street offers an important development opportunity. This block, along with the cinema, should contribute directly to the gateway established at this entrance to Fourth Street. These development opportunities are crucial to establishing the identity of Downtown and entry to its heart in Fourth Street. The current land use occupying the majority of this block is a gas station. The Independent Order of Odd Fellows (IOOF) Hall is also located on this block, at the southwest corner of the Fourth Street/Lawrence Street intersection. This building provides a strong corner presence and should contribute to the sense of entry into Downtown. Eventual redevelopment of the gas station and preservation of the existing Fourth Street development on this block is recommended.

New development at this location should be designed to contribute strongly to this important gateway to Downtown. This is particularly important because of the proximity of this site to State Route 99. New high-quality development at this location will be clearly visible to people traveling on the highway, as evidence of Downtown’s resurgence. New development at this site can also serve as a catalyst project for attracting new investment in Downtown.

### Civic Center Expansion

In late phases and as part of comprehensive expansion of the Downtown Civic Center, a new city hall is proposed at the northeast corner of Third and North Streets. A new library facility is also proposed on Second Street, facing a pedestrian paseo leading to Fourth Street. The existing City Hall and library are proposed to be replaced by surface parking, or potentially a parking structure if necessary in the long term. This parking area could provide a shared parking resource for several critical Downtown destinations. A pedestrian paseo will connect this parking area with the rest of the Civic Core, continuing on to Fourth Street and the Whitmore Mansion. After development of shared parking at this location, a new public plaza is proposed on the current parking lot located east of the Community Center.

The proposed plaza will serve as a focal point for the Civic Center and provide a prominent public gathering space on Fourth Street, which will reinforce the street’s identity as the primary activity zone in Downtown. This concept will also reinforce and build upon other recent investments in the Civic Center area. Finally, the development of a dedicated surface parking area at the current location can be phased over time, thereby increasing feasibility.



### State Route 99 Gateway Commercial Development

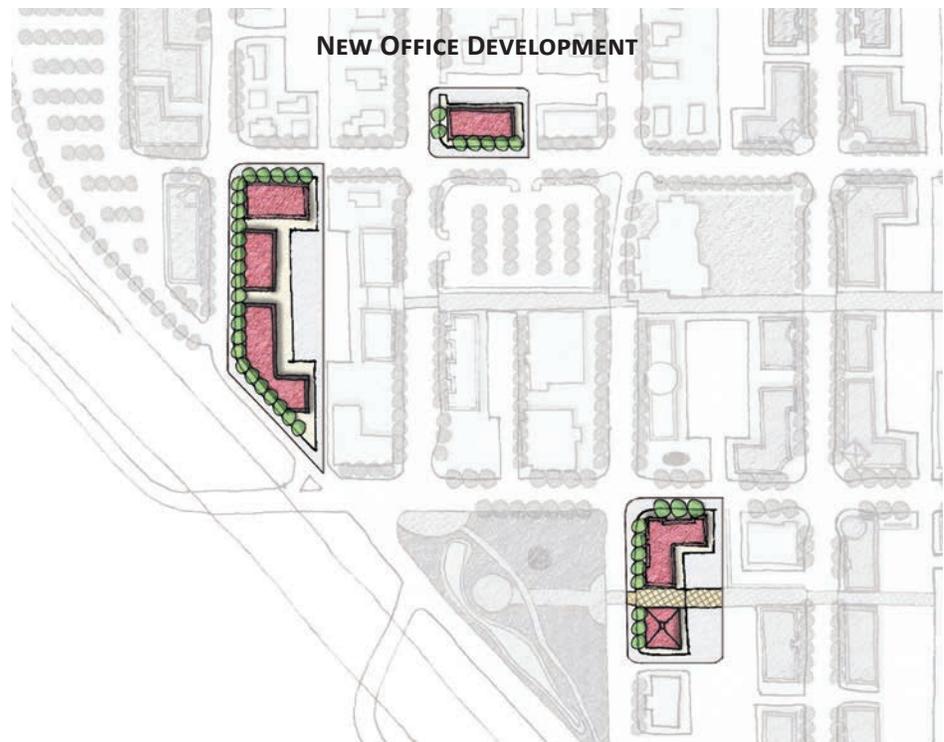
The blocks west of the Civic Center are proposed for new commercial development and other complementary uses. Development occurring in this area should take advantage of its visibility and accessibility from State Route 99 with a mix of office uses and supporting retail, or other commercial uses as appropriate. The stronger opportunity may be in finding a company attracted by highway visibility and convenience, as well as Ceres' strong neighborhoods, which would occupy such a project as a single user, rather than aiming for speculative multi-tenant development dependent upon the City's current, modest office market. Any retail in this area, aside from highway serving commercial similar to existing uses, should not directly compete with Fourth Street businesses. It is recommended that this area be developed as a single project, if possible. Class A office development at this location will bring increased daytime patronage to Downtown's commercial businesses. As in other locations, development at this area should include strong gateway elements that signify entry to Downtown and provide queues to the identity and character within its various districts.



### New Office Development

Office uses are recommended for property fronting onto Central Avenue from the east, as well as new development fronting onto Second and Third Street. Office uses may incorporate mixed use components by including residential uses on upper floors. Office condominiums, live/work units, stand-alone offices, and limited supporting retail uses and services are also appropriate uses.

New office development at this location will provide new spending capacity in Downtown. Offices at this location will also build off of the existing character of this area, which contains some small offices and other lower intensity uses. As with the cinema, office development is more appropriate than development containing residential uses in areas close to State Route 99, due to harmful vehicle emissions.



### Whitmore Mansion Residential

It is proposed that the Whitmore Mansion grounds, which are currently an underutilized resource in the community, be developed into a publicly accessible green space with the Mansion as a centerpiece. Fencing and landscaping should be redesigned to ensure views from Fifth Street toward the Mansion and the Green. New pedestrian friendly residential development will be adjacent to and face the new green space at Whitmore Mansion.

Developing some portion of the Whitmore Mansion grounds with new housing is a key step in achieving the vision and goals, and implements a key concept in the Conceptual Framework. This improvement will benefit Downtown and Ceres in several ways. First, new development at Whitmore Mansion will add new residents to Downtown, which will provide increased support for Downtown businesses. Second, new development can add to the existing residential character in the eastern parts of Downtown by providing a high-quality residential project at a central location. New development can also be designed to help accentuate a key piece of Ceres' history, the Whitmore Mansion.



### Additional Eastern Residential Development

The streets to the east of Fourth Street will potentially accommodate new Downtown residential housing, such as small-lot single-family, townhomes and multi-family apartment buildings. These medium-density housing types are family-friendly and will bring new residents and new life to Downtown.

New multi-family development is encouraged in many areas of Downtown, but particularly on key vacant sites in the eastern areas. Residential infill will help to develop a cohesive residential area within this part of Downtown, and also provide increased demand for Downtown businesses.

### Streetscape Improvements

The existing configuration of Downtown streets offers flexibility and opportunities for innovation in streetscape design. Most Downtown streets have wide rights-of-way, which offer flexibility in design. By reducing the widths of existing travel lanes, additional space becomes available within the right-of-way to add sidewalks, modify existing sidewalks, reconfigure parking, and provide additional streetscape elements, landscaping, bicycle facilities and infrastructure. 10- to 11-foot lanes have been successfully implemented in many downtowns, particularly as trends have moved toward creating multi-modal, walkable environments that support broader goals for sustainability. These goals are consistent with the goals of this Specific Plan, so reduced lane widths are recommended. However, it should be noted that not all street segments in Downtown are suitable for lanes of 10 to 11 feet, particularly when public safety concerns and turning movements of emergency vehicles are an issue. As such, the Downtown Specific Plan does allow for wider lane widths where necessary. Deviations from recommended lane width reductions should be considered on a case-by-case basis and with coordination between the City's Development Services and Public Works Departments.



Bioswales, pavers and street lighting all contribute to streetscape character.



Angled parking, as is proposed to be extended north of North Street on Fourth Street.



Street improved with a median.

Downtown streetscape improvements should be strategically undertaken to achieve the vision and character proposed for Downtown. Streetscape improvements should seek to accommodate pedestrians and vehicles and, in some cases, should provide facilities for bicyclists. Streetscapes should be considered as a resource for the provision of public gathering space in Downtown and provide amenities for pedestrians, such as benches, newspaper racks and pedestrian-scale street lighting. Streetscapes also provide opportunities to manage stormwater by including bioswales at appropriate locations and by increasing the tree canopy in Downtown. Bioswales may not be appropriate in all locations, but are specifically recommended on North and Fifth Streets. Cost and maintenance issues related with bioswales should be evaluated prior to proceeding with construction. Large canopy street trees will also provide needed shade during hot summer months.

Streetscape improvements will assist in accomplishing goals for diversity and unique character, a pedestrian environment, economic viability and sustainability. This will be accomplished through a series of streetscape treatments that provide elegant spaces that are comfortable and enjoyable to pedestrians. Streetscape improvements can increase economic viability by helping to attract new investment to Downtown. Finally, streetscape improvements in certain locations can accommodate new stormwater management infrastructure. Specific streetscape concepts for Fourth Street, Whitmore Avenue, North Street, Fifth Street and Third Street are discussed in Chapter Four, Development Concepts.

### **Downtown Parking**

Accomplishing goals for a multi-modal Downtown requires a comprehensive strategy for parking to ensure it is provided as efficiently as possible. On-street parking is important to businesses and should be preserved throughout Downtown. To complement on-street parking in Downtown, off-street surface parking will be necessary. As shown in the Conceptual Illustrative Plan, it is proposed that surface parking provided by Downtown businesses be located behind buildings. Consolidating parking in the middle of blocks and encouraging development to front directly onto streets will help support goals for a pedestrian-oriented Downtown by creating streetscapes with a sense of enclosure and character, and by facilitating inviting storefronts. Consolidated parking behind buildings can be accessed by existing alleys. Implementation of this concept will help establish a “parking system” for Downtown, particularly for Fourth Street businesses.

Shared parking opportunities also exist in the mid-block areas. Shared parking means that some of the parking spaces can be shared by more than one business or user type. Often parking demand varies between uses and by the time of day. Shared parking would help prevent inefficiencies associated with underutilization of parking. For example, a Fourth Street business that is open only during daytime hours may have empty parking areas at night, just as a nightclub would have empty parking areas in the early morning. Shared parking policies proposed in this Specific Plan would reduce the parking requirement for on-site space requirements if shared parking is provided. To complement these parking resources, it is also proposed that surface parking be provided by the City at strategic locations in the long term. One surface parking area can be provided where the current library and City Hall are located. This can be phased as a part of a comprehensive expansion of the Civic Center, as described earlier in this chapter. It is proposed that the City also develop and manage an additional parking area south of the Turlock Irrigation District (TID) building on Third Street.

These parking strategies are unique to Downtown and directly respond to goals for new development, a pedestrian-oriented Downtown and a multi-modal Downtown.

#### **Circulation Improvements**

A limited number of circulation and roadway configuration improvements are recommended in Downtown. Circulation improvements will be necessary to accommodate the buildout anticipated from the Specific Plan. Circulation improvements in Downtown focus on reducing traffic and intersection delays while also facilitating improvements to pedestrian and bicycle circulation. More detail about circulation improvements is contained in Chapter Eight, Circulation.

An integrated pedestrian circulation system is proposed for Downtown. The system will include complete sidewalks, pedestrian paseos and public plazas. The paseo network, in coordination with plazas, consolidated parking and other streetscape improvements, will be instrumental in the establishment of a walkable downtown in Ceres. Paseos will bisect blocks containing parcels under private ownership and will potentially require property acquisition by the City, dedication, public/private partnerships or other mechanisms. Illustrations and additional details about paseos are provided in Chapter Eight, Circulation. Several bicycle improvements are proposed to facilitate better bicycle circulation in Downtown, including new bike lanes and the dedication of additional Downtown streets as bicycle routes. Pedestrian and bicycle facilities are critical for achieving goals for a sustainable Downtown.



Pedestrian circulation improvements are coordinated with small plazas.

### Park Improvements

Existing and proposed parks are critical to achieving the vision for Downtown. A series of new parks and open spaces is proposed for Downtown. Potential projects include enhancement of existing park spaces, development of new hardscaped plaza spaces, public space provision through private development, and new community parks. Additional details about open space improvements are included in Chapter Nine, Public Facilities, Services and Infrastructure.

New parks and improvements to existing park can help provide new and diverse recreational spaces for Downtown residents, as well as for citywide residents and visitors.



Parks and open spaces create dynamic spaces for downtowns.

### Infrastructure Improvements

Upgrades to existing utility infrastructure are needed in Downtown. Upgrades include improvements to storm drainage, wastewater and water infrastructure. Several above-ground utility wires exist in Downtown, and undergrounding of overhead wires visible from Fourth Street should be prioritized. Undergrounding overhead wires on additional Downtown streets should be explored and coordinated with redevelopment efforts. These efforts should focus on undergrounding poles and wires located along primary circulation routes. The existing overhead utilities in the alleys should remain and be accommodated by new development wherever possible.

### 3 VISION, GOALS & CONCEPT

Infrastructure improvements will ensure that this Specific Plan can be implemented over the long term. Providing adequate infrastructure is key in supporting new development. Infrastructure improvements will also help to ensure that Downtown development can be accommodated in an adequate and efficient manner. Infrastructure improvements in Downtown will also help contribute to citywide systems. By encouraging green stormwater infrastructure at key locations, Downtown will serve as a local and regional model for sustainability.

## DEVELOPMENT CONCEPTS 4

This chapter outlines specific development concepts for public improvements and private development in Downtown Ceres. These concepts are meant to illustrate realistic and achievable projects that are also consistent with the vision and goals for Downtown. The streetscape design and site planning undertaken to develop these concepts was conducted as an iterative process to create the key policies, standards and recommendations in the following chapters of this Specific Plan and the Downtown Streetscape Standards and Design Guidelines in Appendix A.

The illustrations show projects that support the goals and vision described in Chapter Three. Included are proposed streetscape improvements for key streets in Downtown, and a conceptual approach to phasing redevelopment on two blocks adjacent to Fourth Street.

## A. Public Realm Improvement Concepts

This section provides conceptual recommendations for streetscape improvements on key streets in Downtown.

### Fourth Street Improvements

Fourth Street will be the activity and retail corridor and the central heart of Downtown. As such, this street must be handsome, functional and pleasant for pedestrians over its entire length. Improvements proposed include vehicle lane width reduction, sidewalk widening, new trees, conversion of parallel to angled parking, an enhanced bulbout at North and Fourth Streets, and new landscaped mid-block pedestrian crossings. These improvements can be provided through a combination of public investment and private development dedications. Figures 4-1 and 4-2 illustrate how these improvements could be implemented on Fourth Street in both cross-section and plan view.

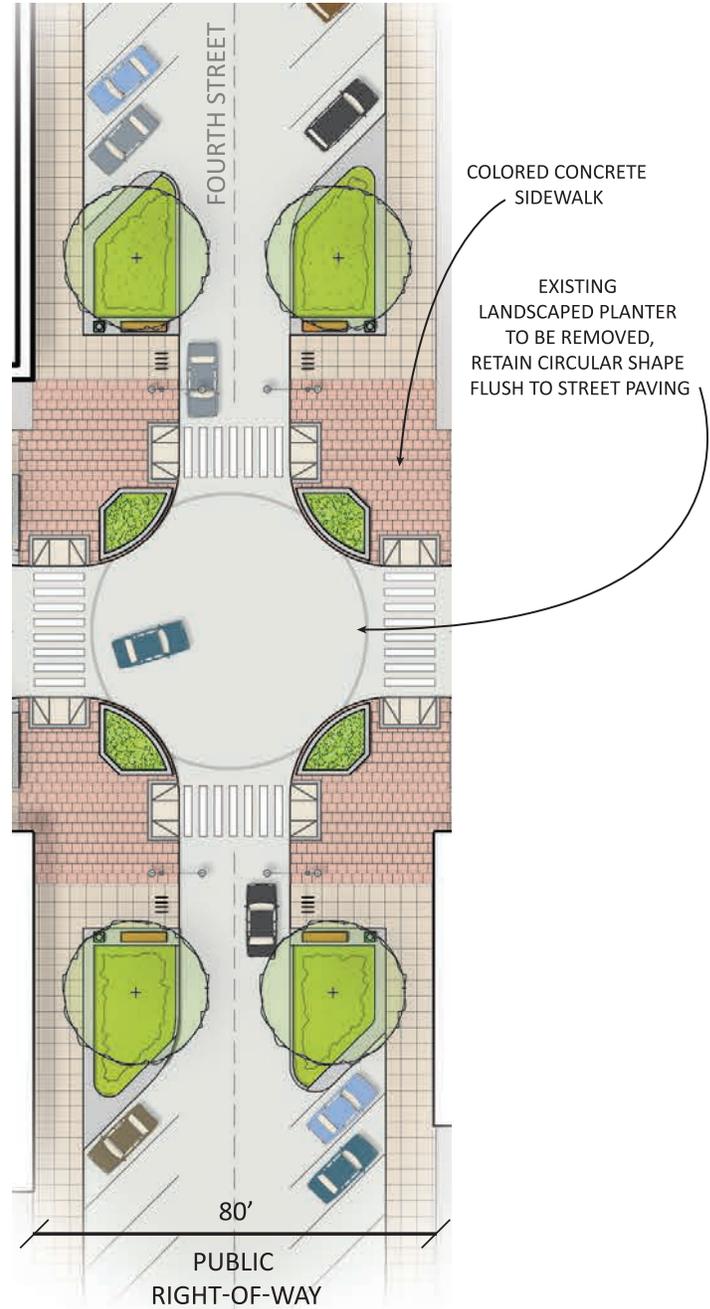
**FIGURE 4-1** Fourth Street Viewing North



The recommendations are specific to Fourth Street and are intended to create an environment that is the “living room” of Ceres and one of the most identifiable streets in the city. These improvements, in coordination with high-quality private development, will ensure Fourth Street becomes a sacred space for Ceres over the long-term. Fourth Street improvements will also help achieve economic viability goals by showing developers the City’s commitment to the Downtown. Fourth Street’s resurgence will also be visible to passers-by on State Route 99. Enhanced gateways will mark entry at the north and south of Fourth Street, further reinforcing the significance of the street within Ceres.

Through elimination of the existing raised roundabout, new bulbouts are shown at intersections, including a special treatment of the Fourth Street/North Street intersection intended to accentuate this important Downtown intersection and the center of Downtown. Bulbouts allow for shorter crossing distances and can also provide additional public gathering space at this intersection. As shown, benches, bicycle racks, street lamps, trash receptacles and special pavement can all be accommodated in this area, while still allowing for clear pedestrian access and ADA compliance. New stop signs will need to be added to this intersection to facilitate these improvements and ensure safe traffic operations.

**FIGURE 4-2** Fourth Street/  
North Street Intersection



### Whitmore Avenue Improvements

Whitmore Avenue will continue as a primary arterial in Ceres, but the street can be improved to become a stately boulevard through Downtown, relate better to Downtown and provide a more comfortable walking and biking environment. After completion of improvements at the Whitmore Interchange project, all northbound exiting traffic will approach Whitmore Avenue, increasing the potential to designate Whitmore Avenue as another key gateway into Downtown.

Lane widths on Whitmore Avenue are wide and could be reduced to provide room for enhancements to the public realm. Landscape improvements are proposed for Whitmore Avenue, including new street trees, a maintained central median with appropriate turn pockets at intersections, and new bicycle facilities. In keeping with the “Green Ceres” concept, it is recommended that medians be landscaped with drought tolerant plants. This type of landscaping is cheaper to maintain and requires less water/fertilizer than traditional turf. Drought tolerant plants in the median will also provide additional permeable surfaces in Downtown. Wayfinding signage is also proposed to further establish entry to Downtown from Whitmore Avenue. It is recommended that median improvements be implemented from Central Avenue and Sixth Street, where primary gateways to the proposed Downtown districts are located. Figure 4-3 shows a conceptual section of Whitmore Avenue.

FIGURE 4-3 Whitmore Avenue Viewing West



Implementing these improvements can help attract investment to Downtown and provide for increased amenities on this primary corridor within Ceres. Streetscape improvements and a well-designed wayfinding and signage system will help lure State Route 99 travelers to the highway commercial development proposed at the northwest portion of Downtown, but also make them consider visiting smaller existing and proposed businesses on Fourth Street.

### Fifth Street Improvements

Fifth Street, from Lawrence Street to Whitmore Avenue, is identified as an historic district by the Specific Plan. The first home in Ceres, the Daniel Whitmore Home, is located on Fifth Street along with the Whitmore Mansion and several other homes that were built around the turn of the twentieth century. This segment of Fifth Street is currently residential, with single-family homes of varying setbacks.

Building on this character, it is proposed that Fifth Street be improved with special landscaping, including new sidewalks, creative designs for open space within the existing 80-foot right-of-way, pedestrian lighting and street trees. Figure 4-4 illustrates a conceptual section of Fifth Street. It is proposed that new streetscape configurations be explored as

**FIGURE 4-4** Fifth Street Viewing North



parcels fronting this street redevelop. For example, it may be appropriate for Fifth Street to “shift” at certain locations or intersections where opportunities to improve the right-of-way exist, particularly where improvements will accentuate properties or features to reinforce the historic character of the street. Any improvements to the street should also include measures to calm traffic speeds and allow provision of a rich street tree canopy. The existing roundabout at Lawrence Street and Fifth Street is proposed to be improved with landscaping and street trees, clearly marking the southern entrance to Historic Fifth Street. Historic signage may also be appropriate at this entry and along Fifth Street.

**North Street Green Connection**

The North Street Green Connection represents an opportunity for Downtown to exhibit sustainability and stormwater management principles in the public realm, as well as connect the major existing and proposed green spaces within Downtown. North Street through Downtown is proposed to be reconfigured with parallel parking throughout by converting the existing angled parking. Changing to parallel parking and requiring extended setbacks will permit bioswales in the right-of-way. Bioswales are provided to manage and cleanse stormwater runoff from the roadway. Please see the Downtown Streetscape Standards and Design Guidelines in Appendix A for more information on

**FIGURE 4-5** North Street Viewing West



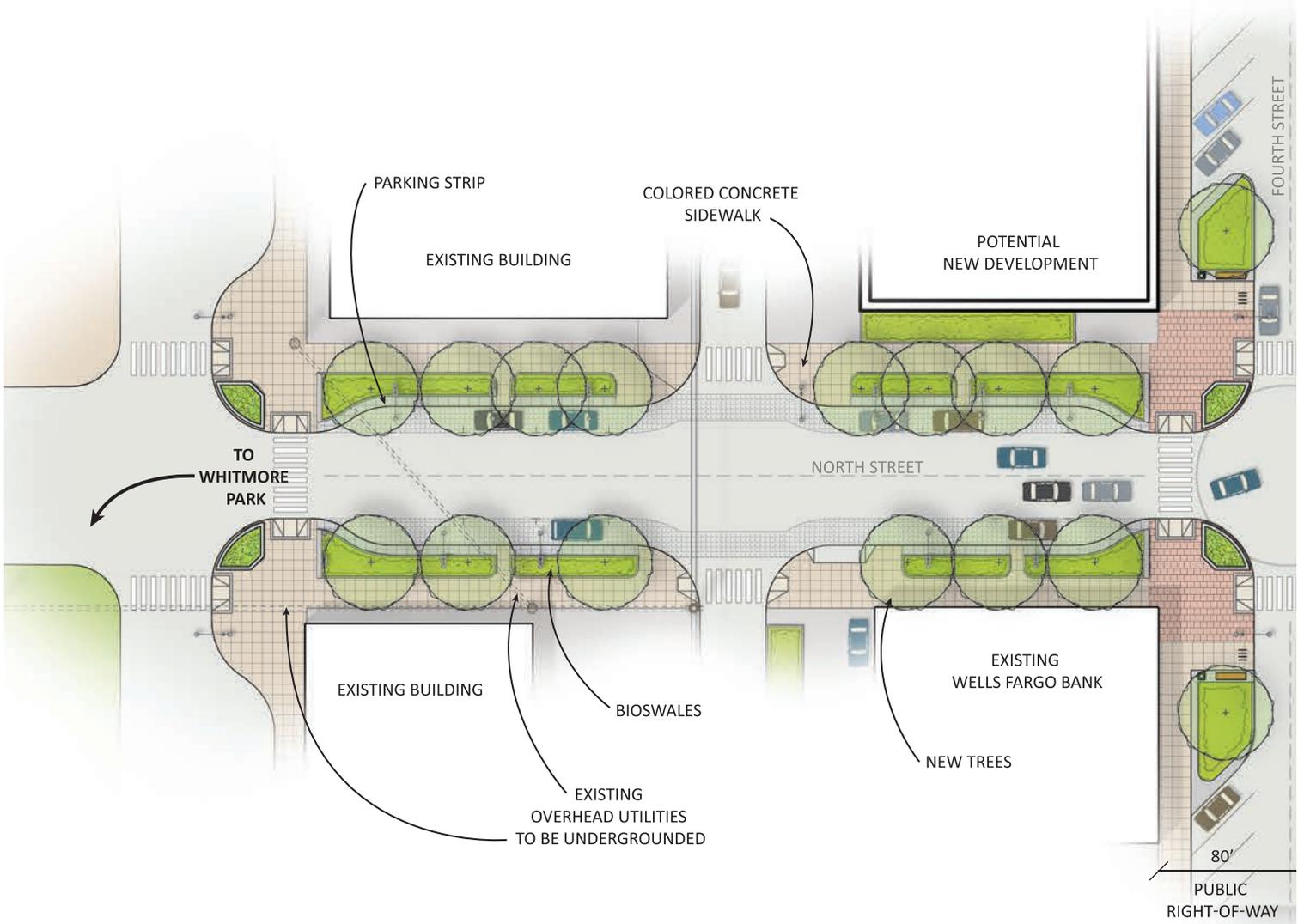
bioswales, including how they can be accommodated in a downtown setting. It is also proposed that sidewalks are widened and large canopy trees are planted. New street lighting is also proposed for North Street. Figure 4-5 shows a conceptual section of North Street.

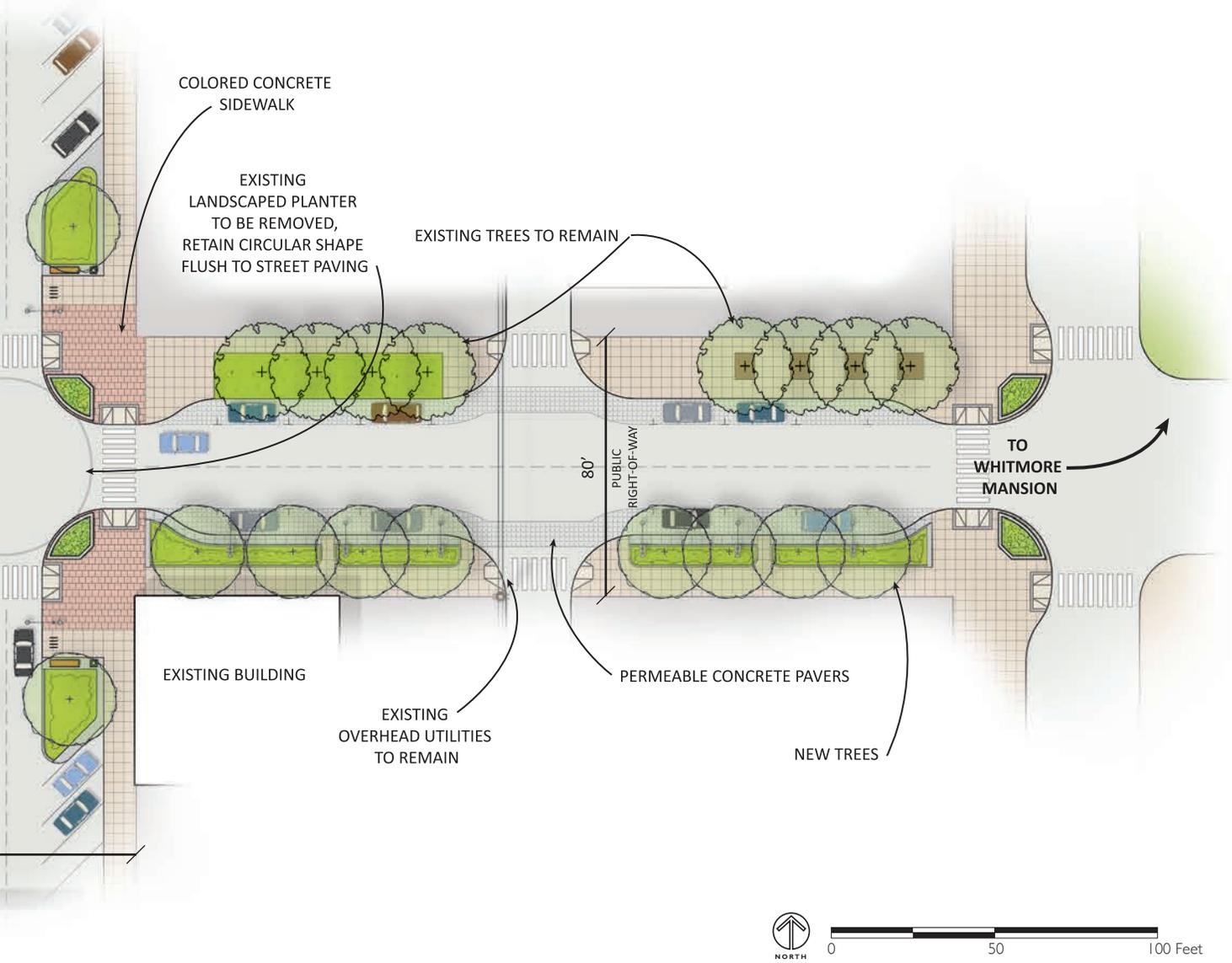
Figure 4-6 shows a conceptual streetscape plan for North Street, which connects Whitmore Park and the proposed green space at the Whitmore Mansion. The figure illustrates how the North Street Green Connection concept could be implemented in the existing street right-of-way, including a redesign of the Fourth Street/North Street intersection. Some of the existing street trees are proposed to be retained on North Street, however new street trees are also proposed. Bioswales are planted with trees and can also accommodate street lighting. Parallel parking areas are designed with permeable concrete pavers that also assist in stormwater management by allowing runoff to be absorbed back into the ground. All overhead utility wires currently running parallel with North Street are proposed to be removed to improve the aesthetic character of this important Downtown street. It is recommended that overhead utilities located in alleys are preserved since they only cross North Street at alley locations.

Improvements to North Street should be prioritized to accentuate this important east-west connector and serve as a sustainable demonstration project for Downtown and the region. It will also add to the diverse and unique character sought for Downtown. Finally, it will provide a link in Downtown between the primary open space in Whitmore Park to a new proposed green space at Whitmore Mansion.

This design is only in a conceptual phase and will need to be refined during a more detailed design process for the street. In this future phase of design, a qualified landscape architect will determine the exact locations for new plantings and other streetscape elements. This process will ensure that no conflicts exist between individual streetscape elements. Technical details about how bioswales function in an urban setting are provided in the Downtown Streetscape Standards and Design Guidelines in Appendix A.

FIGURE 4-6 Conceptual Streetscape Plan for North Street





## **B. Conceptual Redevelopment Phasing**

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The following text and drawings illustrate the conceptual redevelopment of a pair of blocks surrounding Fourth Street. The plan shows the blocks as they could develop over time in three phases. The resulting development is integrated into the existing fabric while providing new office, retail and housing opportunities. The projects shown are relatively modest and feasible to construct, although more expensive podium-parked construction types are shown in the last phase, when future market demand has a better chance of supporting this kind of construction. It is critical that the City, including all departments, work together and in concert with developers to facilitate catalyst projects both financially and through coordinated planning. Specific Plan implementation will be strategically phased as is described later in Chapter Ten, Implementation. To that end, projects and the supporting infrastructure necessary to support them must be coordinated. The conceptual redevelopment shown in the following diagrams is not intended to represent actual projects, only to illustrate the potential for positive change in Downtown.

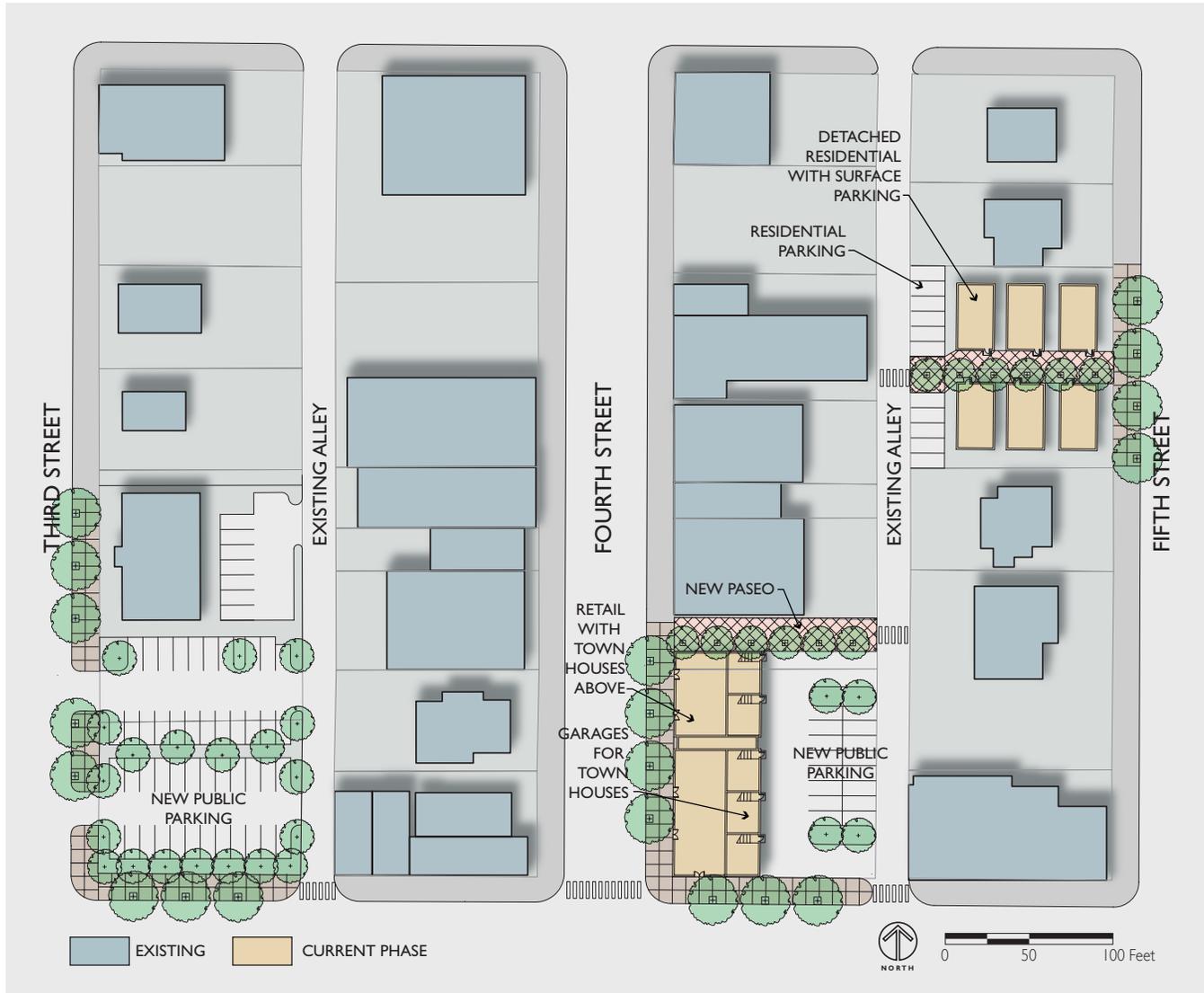
FIGURE 4-7 Mixed Use Development Existing Conditions



**Existing Development**

Figure 4-7 shows the existing development conditions at two typical blocks surrounding Fourth Street. The new Turlock Irrigation District (TID) building is shown with its associated parking areas.

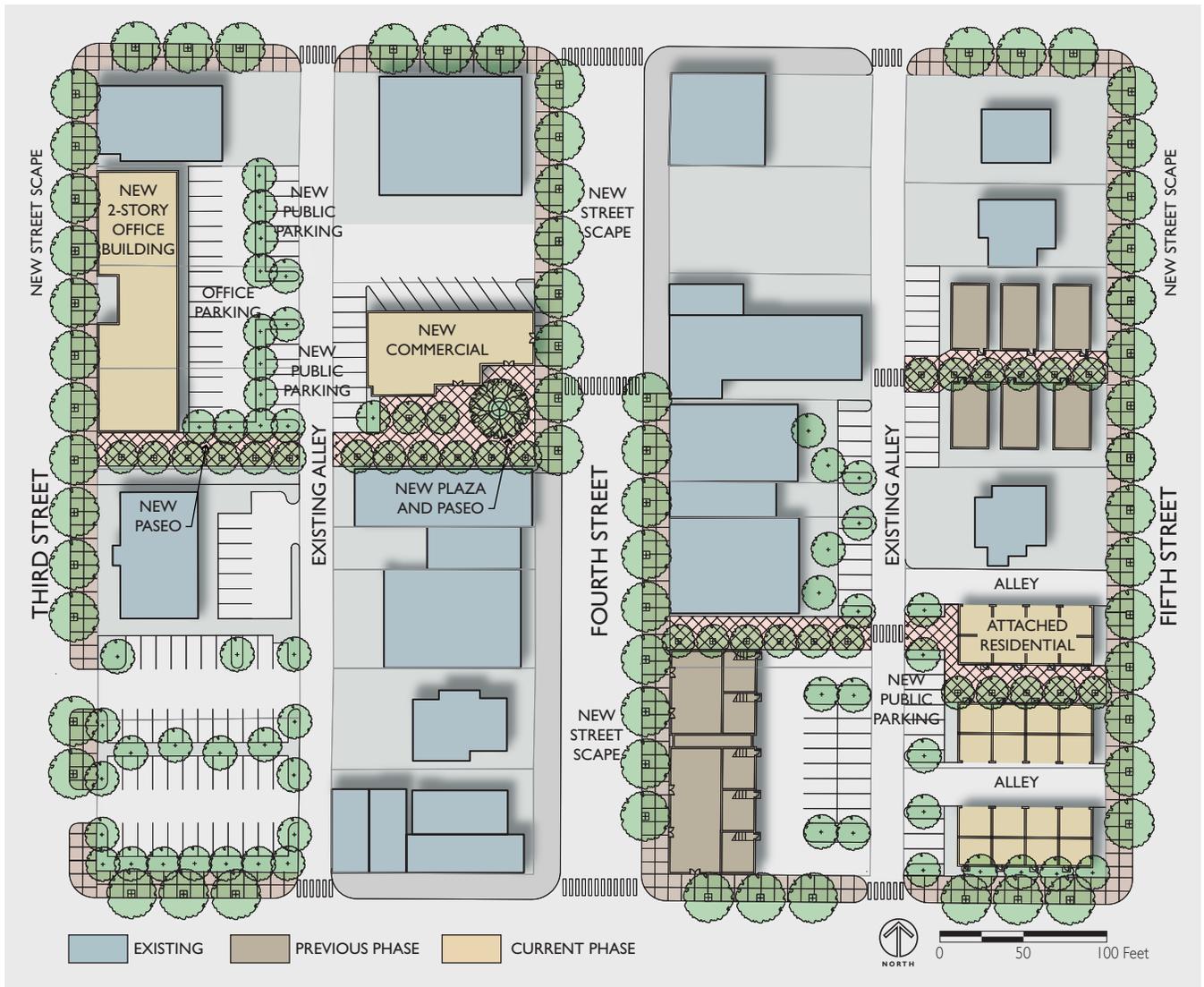
FIGURE 4-8 Mixed Use Development Phase One



**Phase One**

Figure 4-8 shows the same blocks at the end of the first phase of redevelopment. In this phase a new mixed use building with retail storefronts and two story townhomes above are developed at the southeast corner of the eastern block. Garage parking is provided at the ground floor for townhome units and surface parking for retail uses is provided at the rear of the development, accessed by the alley. Sidewalk improvements are provided as a part of this development, along with a new paseo connection to provide a pedestrian access between retail parking areas and storefronts. Additionally, a detached residential project containing six units is developed along Fifth Street. Surface parking is provided along the alley and a paseo provides a connection to Fifth Street. New streetscape improvements are also undertaken as a part of new development. On Third Street, a new public surface parking lot is shown just south of the TID building, which would be coordinated with the existing TID customer parking.

FIGURE 4-9 Mixed Use Development Phase Two



### Phase Two

Figure 4-9 shows Phase Two of the redevelopment of the blocks. New development in this phase includes a two-story office building, a new commercial building and a new townhome development. The office building, oriented toward Third Street, provides secured parking and additional shared public parking along the alley. A paseo is also constructed just south of the office building. A new single story commercial building is designed to engage Fourth Street as well as a new plaza and paseo to the south. A high-end restaurant would be an ideal business to take advantage of the paseo and could provide outdoor seating in the plaza. Eighteen new townhomes are provided on the southeast corner of the eastern block. These townhomes are three-story and include alley loaded garages. New shared public parking is provided along the alley as a part of this development. A pedestrian connection to Fifth Street is provided and serves as the primary front door for the townhomes. Redevelopment in Phase Two also includes streetscape improvements.

FIGURE 4-10 Mixed Use Development Phase Three



**Phase Three**

Figure 4-10 shows the final and third phase of the redevelopment of the blocks. In this phase, two new mixed use podium projects are developed and front onto Fourth Street. Each podium project accommodates secured parking for residents. Additionally, the podium projects each provide shared parking along alleys to help accommodate retail parking demand. Retail space in these buildings is shown as 30 feet in depth to accommodate the podium parking. With this new development, the paseo systems are completed for these blocks. The central paseo connection, which includes the plaza, provides a seamless pedestrian connection across the two blocks. Streetscape improvements are also completed for the two blocks in this phase. It should be noted that podium projects with development over parking may only be achievable in the long-run due to market conditions.

## POLICIES 5

This chapter outlines policies for the Downtown Specific Plan with respect to land use and community design, transportation and circulation, housing, public facilities and services, recreation and cultural resources, housing, preservation, conservation and economic development. The policies evolved from the Specific Plan process, including from input from the community, City staff and the project consultant. These policies are an extension of the General Plan policies that focus on the Downtown or issues relevant to the Downtown. The policies should augment those in the General Plan and provide clear parameters by which City staff and decision-makers can review projects within the Downtown.

More specifically, the policies in this chapter are meant to support the vision, goals and concepts outlined in Chapters Three and Four and the actions recommended in Chapters Eight, Nine and Ten.

## **A. Land Use and Community Design**

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Land use and community design policies support the creation of a vibrant, economically viable and distinct downtown for Ceres. Policies focus on establishing a healthy mix of uses, as well as an appropriate and beneficial relationship between private development and the public realm.

Policy LUD 1. Market Downtown to developers interested in investing in retail, professional office, larger office centers and residential development in Ceres.

Policy LUD 2. Ensure that land uses on Fourth Street are primarily mixed use with ground-floor retail and upper-floor office or residential uses.

Policy LUD 3. Encourage office uses along Second Street and portions of Third Street not associated with the Civic Center.

Policy LUD 4. Encourage residential development that respects the historic resources of Fifth Street.

Policy LUD 5. Encourage residential development of varying types along Fifth, Sixth, Seventh, Eighth and Ninth Streets.

Policy LUD 6. Encourage multi-family and mixed use housing in the Plan Area as a means to bring increased vitality to Downtown and provide additional affordable housing options.

Policy LUD 7. Reinforce and enhance the pedestrian-oriented scale and character of new development and public improvement projects within the Plan Area.

Policy LUD 8. Provide adequate pedestrian amenities, such as street trees, benches, pedestrian-level lighting, newspaper racks and waste receptacles as areas are improved. These facilities and amenities should be provided so as not to conflict with pedestrian circulation or distract vehicles.

Policy LUD 9. Ensure that new development engages the street by being built to or along parcel lines when not in conflict with a Public Utility Easement and by ensuring architecture that provides visual interest and transparency. To the extent feasible, new development should provide amenities on the street that can be accessed by the public, such as small plazas, benches or landscaping.

Policy LUD 10. Create opportunities for interaction between development and public spaces, particularly along Fourth Street.

Policy LUD 11. Create or encourage significant architectural elements or gateway elements at points of entry to Downtown, both in public improvements and through architectural features and site planning related to private development. This is particularly important for gateway and catalyst projects meant to establish momentum for Downtown’s revitalization.

Policy LUD 12. Encourage development to include special architectural features at prominent corners, such as the intersections of Fourth Street and El Camino Avenue, Fourth Street and North Street, and Fourth Street and Whitmore Avenue. Special architectural features should consist of a vertical tower element including colors and finishes, a diagonal wall or “chamfer” facing the corner, a dedicated public space at the corner, or a combination of any of these.

Policy LUD 13. Encourage development with an appropriate orientation and massing to frame and provide enclosure to streets and public spaces.

Policy LUD 13. Where appropriate, encourage new development to include publicly accessible plazas and open spaces.

Policy LUD 14. Ensure that new retail and residential development respect adjacent residential development, particularly single-family homes exhibiting architectural integrity.

Policy LUD 15. Encourage development that minimizes impervious surfaces.

Policy LUD 16. Encourage cooperation between public and private entities in achievement of catalyst projects that will help to “kick-start” Downtown revitalization. All City departments should endeavor to assist in accomplishing this policy.

## **B. Transportation and Circulation**

Transportation and circulation policies support the creation of a pedestrian-oriented downtown that remains supportive of bicycle and vehicular circulation. This section also includes policies for parking that similarly support a pedestrian-oriented downtown.

Policy TRAN 1. Provide, widen and improve sidewalks on both sides of all streets within Downtown.

Policy TRAN 2. Provide Class II bicycle lanes on Whitmore Avenue and Class III bicycle routes on Central Avenue, Sixth Street, Ninth Street, Park Street, Lawrence Street and El Camino Avenue. Please refer to Chapter Eight for a map of proposed bicycle lanes and routes in the Specific Plan Area.

Policy TRAN 3. Create a system of public pedestrian paseos to connect key destinations within Downtown to Fourth Street. Consider property acquisition, development incentives and public/private partnerships to facilitate construction of paseos.

Policy TRAN 4. Reconfigure the North Street/El Camino Avenue/Second Street intersection to minimize conflicts and encourage motorists to travel Downtown.

Policy TRAN 5. Encourage development that provides publicly shared parking areas behind buildings, accessed by the existing alley system.

Policy TRAN 6. Preserve on-street parking that provides a buffer between pedestrians and vehicular traffic.

Policy TRAN 7. Ensure special landscape features in the right-of-way on Fifth Street as properties redevelop. For example, the street could shift at certain locations to accentuate key properties and to help highlight the overall historic character of the street.

Policy TRAN 8. Remove the raised traffic circle at the Fourth Street/North Street intersection to allow unobstructed travel and clear circulation for visitors, shorter crossing distances, and larger bulbouts that can provide additional public gathering and landscaping space.

Policy TRAN 9. Allow parking charges to be separated (unbundled) from the residential rent for multi-family units to provide an incentive for households to minimize their vehicle ownership.

Policy TRAN 10. Allow tandem parking opportunities that meet City standards in new commercial and residential development.

Policy TRAN 11. Encourage developers to provide parking spaces that can be reserved for car-share services.

Policy TRAN 12. Provide reductions in parking requirements for developers that provide public or shared parking. More detail about shared parking can be found in Chapter Three, Vision, Goals and Concept. Specific parking reductions are included in Chapter Seven, Land Use Regulations and Development Standards.

Policy TRAN 13. Include traffic calming measures to slow traffic in the Specific Plan Area, including expanded bulbouts at intersections, lane width reductions, and other similar streetscape improvements. Slower vehicle speeds will ensure increased safety for pedestrians and mitigate noise impacts associated with increased vehicular activity.

## C. Public Facilities and Services

Public facilities and services policies focus on the provision of adequate, sustainable utility infrastructure systems. This section also provides policies for stormwater management and safety measures in private developments

Policy PUB 1. Ensure that all utility facilities have adequate capacity to accommodate new development.

Policy PUB 2. If appropriate and after a nexus study is undertaken, develop an impact fee mechanism that allows the costs of developing utility infrastructure improvements to be borne by new developments in an equitable manner.

Policy PUB 3. Encourage on-site stormwater management and permeable surfaces to minimize the impact of new development on drainage facilities.

Policy PUB 4. In areas that do not have fire hydrants located at the standard spacing of every 300 foot, require Automatic Fire Sprinkler Systems in buildings with over 5,000 square feet of floor-area.

Policy PUB 5. Encourage the replacement and renovation of infrastructure systems to employ Best Management Practices (BMPs) to ensure that no negative environmental effects occur.

Policy PUB 6. Where appropriate, encourage bioswales in street right-of-ways to help manage stormwater runoff from roadways and to provide additional landscaping. Prioritize bioswales along North Street. For public bioswale projects, City staff should fully consider cost and maintenance costs prior to proceeding.

## **D. Recreational and Cultural Resources**

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Recreational and cultural resources policies support the creation of a downtown that has adequate and meaningful public open space that is accessible to residents inside and outside the Plan Area. Additionally, this section presents policies that are supportive of the preservation and celebration of Ceres' historic and cultural resources.

Policy REC 1. Consider public/private partnerships or other mechanisms to open some portion of the Whitmore Mansion property to the public.

Policy REC 2. Consider improvements to the existing Whitmore Park to include enhanced public plaza space and community gardens. Also consider a berm or other structure at the western edge to minimize noise and visual intrusion from State Route 99. Additional potential locations for community gardens include the proposed Water Tower Park, Whitmore Mansion open space, or (on a temporary basis) vacant parcels owned by the City.

Policy REC 3. Redevelop the current water tower site as a small neighborhood park.

Policy REC 4. Develop a public plaza east of the Community Center that fronts directly onto Fourth Street. This improvement should be implemented in late phases as new development facilitates increased feasibility of new parking investments. Improvements should be coordinated with parking improvements to ensure that adequate parking is available for the Ceres Community Center.

Policy REC 5. Consider allowing or facilitating community gardens on parcels throughout Downtown, particularly on vacant, irregularly-shaped parcels that are not expected to redevelop in the short-term.

## **E. Housing**

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Housing policies focus on the preservation of existing housing units where appropriate and encourage the development of a range of new housing opportunities within the Plan Area. Housing policies also focus on providing housing affordable to families and individuals within all income levels.

Policy HOUS 1. Encourage the development of a variety of housing types, including traditional single-family homes, small-lot single-family homes, townhomes, condominiums, apartments (stand-alone and as components of mixed use buildings) and live/work units.

Policy HOUS 2. Accommodate a range of housing opportunities that are affordable to community members of all incomes.

Policy HOUS 3. Encourage the facilitation of a Low Income Housing Tax Credit (LIHTC) project in Downtown.

Policy HOUS 4. Encourage the development of higher-density housing to add demand for Downtown retailers and services.

Policy HOUS 5. Encourage redevelopment in the Specific Plan Area that results in no net loss of housing units.

Policy HOUS 6. Encourage a two to one replacement of any existing housing units displaced by redevelopment efforts in Downtown, in areas where housing units are permitted.

Policy HOUS 7. Encourage the retention of high quality single-family units in areas designated for residential use.

Policy HOUS 8. Strive for balanced growth of housing and commercial uses in Downtown.

Policy HOUS 9. Where possible, provide assistance to existing residents displaced by redevelopment who wish to find new housing in Downtown.

## **F. Preservation**

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Preservation policies focus on the preservation of cultural resources within Downtown. Cultural resources include identified historic structures and significant landmarks.

Policy PRES 1. Preserve and enhance the Whitmore Home on Fifth Street by ensuring that new development does not prevent views to the structure.

Policy PRES 2. Work with owners of the Whitmore Mansion property to renovate the Mansion grounds to become a key open space in Downtown. Renovation of the grounds might include alterations or removal of surrounding fencing, creative landscaping, or the addition of intermittent entrances, all of which are intended to provide a new Downtown open space and enhance the visibility of the Whitmore Mansion.

Policy PRES 3. Enhance the Ceres Water Tower by considering the development of a small neighborhood park near the structure to celebrate this structure.

Policy PRES 4. Require architectural assessment of potentially historic structures prior to redevelopment or demolition that may impact them.

Policy PRES 5. Where appropriate, ensure that new development protects and respects important historic properties within the Specific Plan Area through design features, setbacks, and other development features, as well as through the implementation of Development Standards and design guidance in Chapter Seven and Appendix A of this Specific Plan.

Policy PRES 6. Require developers to provide an assessment of historical, geological, archaeological and paleontological resources on or adjacent to a project site prior to construction for all projects involving sub-grade disturbance or excavation.

## G. Conservation

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Conservation policies focus on preserving biological and natural resources within Downtown. Resources include plant and animal species.

Policy CON 1. Retain mature native trees in the design of new developments, as determined appropriate by an arborist. If retaining existing trees in new development proves infeasible, please see Chapter Seven, Development Standards, for replacement requirements and other Downtown landscaping standards.

Policies CON 2. Require the use of drought-tolerant plants in new developments. Refer to the Downtown Streetscape Standards and Design Guidelines in Appendix A for a list of suggested plants for private development in Downtown. Street tree recommendations are also provided in that document.

Policy CON 3. Require that prior to development, new development shall have a biologist conduct a site survey to properly identify elderberry shrubs on site that could be adversely affected by development. If necessary, the biologist shall identify appropriate mitigation. It should be noted that it is highly unlikely that these resources exist in this developed area.

## H. Economic Development

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Economic development policies focus on the commercial revitalization of Downtown Ceres, strategic recruitment of businesses and job creation.

Policy ED 1. Brand and market Downtown to facilitate new investment and attract visitors. Provide financial resources to the Redevelopment Agency in the City budgets to support this policy.

Policy ED 2. Seek destination commercial and institutional uses that increase economic activity and promote additional pedestrian activity.

Policy ED 3. Encourage appropriate businesses within Ceres and from elsewhere to relocate to or expand within Downtown.

Policy ED 4. Support and encourage the attraction of a medium-sized cinema within Downtown.

Policy ED 5. Allow for the temporary closure of Downtown streets for special events to help promote Downtown, such as farmers' markets, street fairs and other community events.

Policy ED 6. Encourage mixed use, retail and office developments that provide a range of jobs within Downtown.

Policy ED 7. Provide financial resources from the City in support of catalyst projects that will help initiate Downtown revitalization.

Policy ED 8. Seek public-private partnership opportunities that assist in supporting the revitalization efforts set forth for Downtown in this Specific Plan.

Policy ED 9. Invest public resources at a level sufficient to support successful implementation of the vision, goals and recommendations outlined in this Specific Plan.

Policy ED 10. Coordinate and work with regional economic development entities to solicit grants and to identify Ceres as a future economic development engine for the region.

# 5 POLICIES

## LAND USE 6

This chapter discusses the land use concept and provides land use designations for Downtown, including associated development intensities that will apply in Downtown. In order to support the goals of the General Plan and this Specific Plan, Downtown is designated with several of the existing General Plan Land Use Designations.

## **A. Districts Concept**

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This section describes the land use districts identified for Downtown, highlighting the predominant uses envisioned for each area. Figure 6-1 shows the Downtown districts.

### **Fourth Street Mixed Use District**

Fourth Street will be the commercial heart of Ceres. New development is encouraged to be mixed use, with ground floor retail and upper floor residential or professional office. Heights are envisioned as two and three stories, with parking provided on-street and in mid-block public and private parking lots behind buildings. This use type will be located along the full extent of Fourth Street. Anchor uses, such as a cinema, will help enliven Fourth Street at night and on weekends.

### **Civic Center/Office District**

The Civic Center/Office District will collectively create a revitalized civic core for Downtown Ceres. Civic uses will be located along both streets, between North Street and Magnolia Street. The civic core will be surrounded on the north and south by supporting office uses, existing residential development and Whitmore Park.

### **SR 99 Gateway Commercial District**

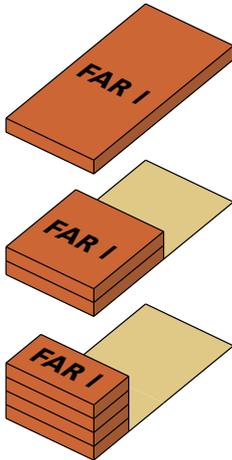
The SR 99 Gateway Commercial District will be generally defined by office and other appropriate commercial buildings that take advantage of their proximity to the freeway. The three blocks currently bounded by Central Avenue on the east, El Camino Avenue on the west, and Whitmore Avenue on the north are collectively seen as a new development opportunity for Downtown. This important development opportunity will only be possible through the consolidation of several parcels and the potential abandonment of Poplar Street and Magnolia Street west of Central Avenue. Mixed use development with ground-floor retail and upper-floor office is envisioned for the southeast corner of Whitmore Avenue and Central Avenue.

### **Eastern Residential District**

The predominant land use in the remainder of Downtown, east of Fourth Street, is residential. It is envisioned that this area will be composed of a mix of residential use types, including single-family homes, townhomes and multi-family residential uses. The historic uses located in this area, including the Whitmore Mansion, Whitmore Home and Ceres Water Tower will remain as key landmarks. New development will be required to respect the historic scale and character of these landmarks through the implementation of special development standards (setbacks, height restrictions, etc.) and through implementation of the design guidelines in Appendix A.

FIGURE 6-1 Districts





An example of how FAR can be distributed.

## B. Land Use Designations

The following land use designations are based on the Districts Concept described in the previous section and will apply within the Specific Plan Area. Residential densities are stated as the number of housing units per gross acre. Development is required within the density range, both maximum and minimum, as stipulated in the land use designation. Maximum floor area ratios (FAR) are also provided for many of the land use designations. FAR is measured as the amount of gross floor area divided by the total parcel area.

The standards outlined in Chapter 7 may limit attainment of maximum densities. Existing land uses that are not consistent with the regulations associated with these land use designations are permitted to continue as legal nonconforming uses. The majority of the land use designations below come from the City’s General Plan. Additionally, two new designations have also been created to respond more appropriately to the vision established for Downtown. New land use designations include Downtown Mixed Use (DMU), Downtown Office (DO) and Downtown Residential (DR). Figure 6-2 shows the land use designations for the Specific Plan Area.



Example of a mixed use building with residential density of 40 dwelling units per acre.

### Downtown Mixed Use (DMU)

The Downtown Mixed Use designation provides for a full range of ground-floor retail and service uses, including apparel stores, restaurants, specialty shops, entertainment uses, bookstores and other similar retail. It also allows for financial services and upper-floor residential and professional office uses. This designation provides for residential uses at densities between 10.0 to 40.0 dwelling units per gross acre (du/gross acre). 40 dwelling units per acre is a residential density that will allow for three-story mixed use development with podium parking, which is especially supportive of the walkable environment desired for Downtown. It should be noted that 40 dwelling units per acre may not be achievable in the short-term due to market conditions, but this density would be an appropriate scale for Downtown if the market will support this development type in the long run. Residential development as a mixed use in conjunction with nonresidential development is permitted and encouraged. However, it should be noted that not all residential development, such as live-work units, are appropriate at densities as high as 40 dwelling units per acre. Development permitted on properties designated DMU will typically be mixed use with ground-floor retail and upper-floor residential or professional office. However, if appropriate, residential development may be allowed on ground floors. Nonresidential development in this designation shall not exceed an FAR of 3.0.

FIGURE 6-2 Land Use Designations



### **Highway Commercial (HC)**

This designation provides for uses designed to serve motorists traveling along State Route 99 at or near interchanges that are convenient and safe for such uses. This designation is also intended to provide locations for uses that depend on high visibility from the freeway. Allowable uses in this designation include service stations hotels/motels, restaurants, and other similar uses primarily oriented toward visitors and travelers. Development in this designation shall not exceed an FAR of 0.5.

### **Downtown Office (DO)**

This designation provides for medical, professional, administrative, general office and limited commercial service uses such as restaurants, dry cleaners and other similar uses that are intended to serve the employees and clientele of the office uses in the immediate surrounding areas. Residential development is permitted at densities of 5.0 to 25.0 du/gross acre. Residential uses allowed in the Downtown Residential designation are allowed in this designation when found to be compatible with existing and future uses. Development in this designation shall not exceed an FAR of 1.0. Parking structures shall be excluded in calculating gross floor area.

### **Community Facilities (CF)**

This designation is applied to the city's major public and private facilities and institutional uses. Most common are public safety facilities such as fire stations. The designation of any future public or institutional site that has not been acquired shall not be construed to limit the existing or future use of the designated land in any way. The predominant land use designation surrounding any property designated for a future community facilities use shall be used to determine the potential use of the property prior to its acquisition by a governmental agency or private institution.

### **Downtown Residential (DR)**

This designation provides opportunities for attached single-family and multi-family residential uses at relatively high densities. For attached single-family and multi-family housing at these densities, lot size is far less important than overall density. Residential densities within the designation range from a minimum of 10.0 to a maximum of 30.0 du/gross acre. Maximum residential densities are encouraged to provide a greater 24-hour population in Downtown, as well as to provide vibrancy on the street and greater support for Downtown businesses. Housing types permitted in this designation include single-family dwellings, townhouses, condominiums, duplexes, triplexes and apartments. Residential developments on properties designated Downtown Residential will typically consist of a mixture of attached single-family dwellings and townhouses and apartments at high densities.

**Schools (S)**

This designation is applied to existing and proposed public schools. There are no additional areas designated for proposed public schools in the Plan Area. If this Specific Plan is amended or revised to designate proposed public schools in the Plan Area, the land use designation shall not be construed to limit the existing or future use of the designated land in any way. The predominant land use designation surrounding any property designated for a future school shall be used to determine the potential use of the property prior to its acquisition by a public school district.

**Parks (P)**

This designation is applied to existing and proposed public parks. The designation of any future park site that has not been acquired shall not be construed to limit the existing or future use of the designated land in any way. The predominant land use designation surrounding any property designated for a future park shall be used to determine the potential use of the property prior to its acquisition by the City of Ceres.

**C. Specific Plan Buildout**

Table 6-1 below shows estimated buildout of the Specific Plan by land use category.

**TABLE 6-1** Estimated Specific Plan Buildout

	Existing (2009)	Specific Plan Buildout (2024)	Difference
Commercial/Retail	134,000 square feet	200,000 square feet	66,000 square feet
Cinema	0	40,000 square feet	40,000 square feet
Residential	220 Units	715 Units	495 units
Office	53,000 square feet	140,000 square feet	87,000 square feet
Civic	92,000 square feet	136,000 square feet	44,000 square feet
School District Facilities	122,000 square feet	122,000 square feet	0
Churches	28,000 square feet	28,000 square feet	0

Note: New commercial/retail square footage listed above reflects replacement of over half of the existing stand-alone retail square footage with mixed-use development.

## 6 LAND USE

# LAND USE REGULATIONS & DEVELOPMENT STANDARDS 7

This chapter provides land use regulations and development standards for all future development in Downtown.

## A. Land Use Regulations and Development Standards Structure

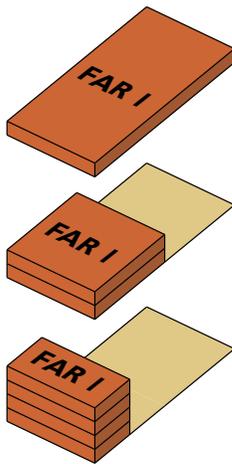
The standards established in this chapter are meant to facilitate the type of development that corresponds to the vision and goals for Downtown, and apply only within the Downtown Specific Plan Area. All new development, remodeled exteriors and new signage are subject to design review by City planning staff upon architectural and site plan approval.

Please refer to the following zoning classifications in the Ceres Zoning Ordinance for each district:

- **Downtown Mixed Use.** Refer to C-2 (Community Commercial).
- **Civic Center/Office.** Refer to A-P (Administrative Professional)
- **SR 99 Gateway Commercial.** Refer to H-1 (Highway Commercial).
- **Eastern Residential.** Refer to R-4 (Medium High Density Multiple Family Residential)

If provisions in the Specific Plan and the Zoning Ordinance are in conflict, the provisions in the Specific Plan should be applied.

Some of the existing uses in Downtown may be considered “legal nonconforming” per the new use regulations in this chapter. Nonconforming uses are those that would not be allowed under the use regulations in this chapter, but existed legally under a previous code (Ceres Zoning Code). As discussed in Chapter 18.48.040 (Group A Nonconforming Lots, Buildings and Uses) of the Ceres Municipal Code, nonconforming uses that are not detrimental in the zone may be continued and under certain conditions altered or enlarged. The Development Standards are organized by district and address the following.



An example of how FAR can be distributed.

### Land Use Regulations

This section provides a matrix of allowable and conditionally permitted uses for each district.

### Development Standards

This section provides development standards for each district.

- **Residential Density.** The maximum number of dwelling units per gross acre is provided for each district.
- **Floor Area Ratio (FAR).** FAR is defined as the floor area of the building divided by the total project site area. FAR includes all floors of a building but does not count parking even if inside the building. Maximum FAR is provided for each district.

- **Height.** The maximum height allowed for buildings for each district.
- **Setbacks.** The minimum and maximum setbacks required along the front, side, exterior side, and rear of a site.
- **Parking.** The minimum and maximum required off-street parking spaces per square foot of commercial development or per residential dwelling unit. Parking ratios for commercial uses in Downtown are lower than citywide parking ratios to support Specific Plan goals for sustainability and walkability. Shared parking, on-street parking, parking areas, and potential future parking structures anticipated in Downtown will also decrease the need for on-site surface parking.
- **Parking Lots.** Requirements for the location of parking areas.
- **Landscaping.** Required landscaping for new development.
- **Access.** Requirements for vehicular circulation.

#### ***Whitmore Avenue Frontage Standards***

This section provides special street frontage requirements for development adjacent to Whitmore Avenue.

#### ***North Street Frontage Standards***

This section provides special street frontage requirements for development adjacent to North Street.

#### ***Historic Fifth Street Frontage Standards***

This section provides special street frontage requirements for development adjacent to Historic Fifth Street.

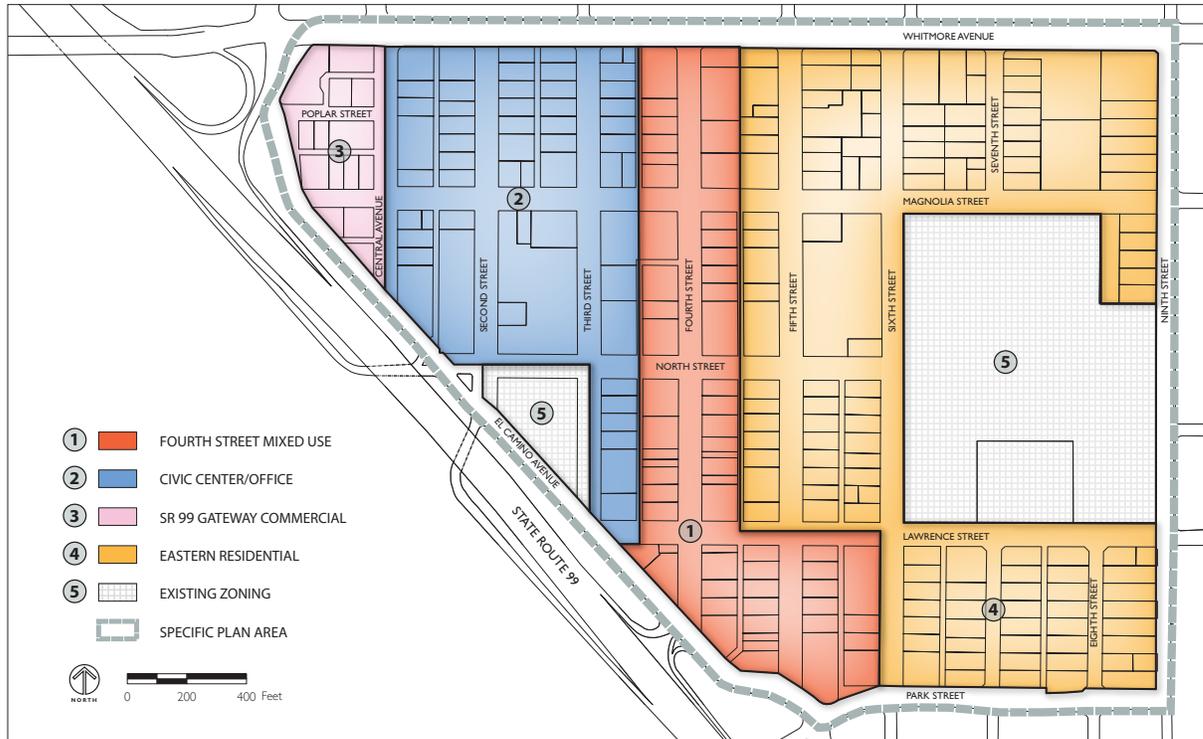
#### ***Definitions***

This section provides a list of terms and definitions to clarify the land use regulations and development standards.

## **B. Land Use Regulations**

This section provides land use regulations for each Downtown district. Figure 7-1 shows the Districts to which the standards apply. Table 7-1 identifies permitted (P) and conditionally (C) permitted land uses in each district. Uses without a “P” or “C” are not permitted. Buildings, structures, and land shall be used and hereafter be erected, structurally altered or enlarged in the according to the uses listed by district in Table 7-1, plus such other uses as the Commission may deem to be similar and are not more obnoxious or detrimental to the public health, safety and welfare.

**FIGURE 7-1** Development Standard Districts



**TABLE 7-1** Land Use Regulations

Key: P = Principal Use C = Conditional Use Permit

Upper Floor Uses	Fourth Street Mixed Use	Civic Center/Office	SR 99 Gateway Commercial	Eastern Residential
Multi-Family Residential	P	P		P
Single-Family Dwellings		C		P
Bed and Breakfast	C			P
Hotels and Motels	C	C	C	
Child Care Facilities	C	P	P	
Offices, Professional	P	P	P	
Offices, Service	C	P	P	
Parking Facilities	C	P	P	
Live/Work Units	C	P	P	

**TABLE 7-1** Land Use Regulations (Continued)

Key: P = Principal Use C = Conditional Use Permit

Ground Floor Uses	Fourth Street Mixed Use	Civic Center/ Office	SR 99 Gateway Commercial	Eastern Residential
Single-Family Dwellings				P
Duplex Homes				P
Multi-Family Residential	C	C		C
Retail, Neighborhood	P	C	C	C
Retail, General	P	C	C	
Retail, Regional		C	P	
Restaurants	P	C	P	
Bars and Nightclubs	C		C	
Bed and Breakfast	C			P
Hotels and Motels	C	C	C	
Government and Professional Office	P	P	P	
Offices, Service	C	P	P	
Personal Services	P	P	P	
Banks, Retail	P	P	P	
Theaters and Auditoriums, Public	C	P	C	
Libraries and Museums	C	P	C	C
Commercial Recreation, Indoor	C	P	P	
Meeting Facilities	C	C	C	
Parks and Recreational Facilities	P	P	P	P
Parking Facilities	C	C	C	
Places of Worship	C	C	C	C
Clubs and Lodges		C	C	C
Hospitals			C	
Medical Clinics and Labs	C	C	P	
Medical Offices	P	P	P	
Schools, Public or Private	C	C	C	
Day Care Facilities	C	P	C	C
Social Assistance Services	C	C	C	C
Public Safety Facilities	P	P	C	C

### C. Fourth Street Mixed Use District

The following standards apply to all development within the Fourth Street Mixed Use District.



#### Residential Density

- Maximum dwelling units per gross acre for residential development is 40.0.

#### Floor Area Ratio (FAR)

- Maximum FAR for commercial and retail uses is 3.0.

#### Height

- Buildings shall be no more than three stories tall.
- Ground floor commercial space shall be at least 14 feet and shall not exceed 20 feet, as measured from floor-to-floor. Upper stories shall not exceed 16 feet, as measured from floor-to-floor.

#### Setbacks

- Front: Where no public utility easements interfere, development must be built to the property line for 70 percent of the site. If a public utility easement does exist, buildings should be built as close to the property line as is feasible.
- Side: None required.

- Exterior Side: None required.
- Rear Setback: 5 feet from rear property line or if an alley exists, 20 feet to alley right of way.

### **Parking**

- Multi-family Residential: 1 space per unit for 1-bedroom units and studios. 0.5 additional parking spaces per each additional bedroom.
- Single-family Residential: 1 space per unit.
- Commercial: 1 space for every 450 square feet of retail floor area.
- Office: 1 space for every 375 square feet of floor area, not including areas used expressly for storage.
- Bicycle Parking: Bicycle parking spaces shall be provided at a rate equal to 5 percent of the total required parking spaces for commercial uses and 10 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).
- Parking Reduction: Reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an irrevocable access and public parking easement should be filed to ensure future owners cannot block access to shared parking areas.

### **Parking Lots**

- New parking areas are strongly encouraged to be located behind buildings and shall not be located within front setbacks or exterior side setbacks.

### **Landscaping**

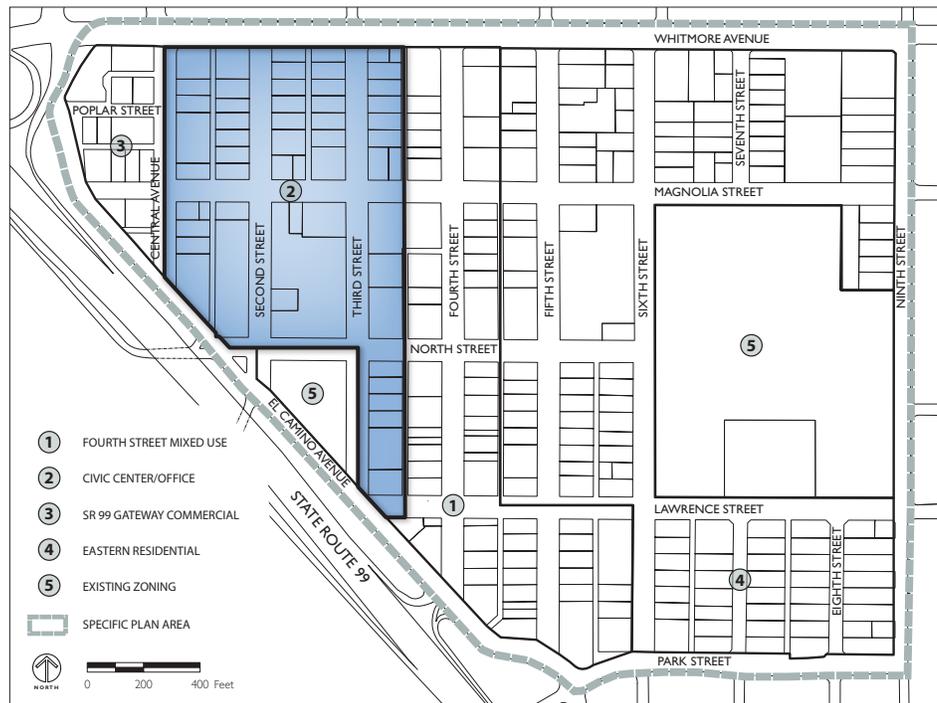
- Development shall maintain landscaping.
- Native and other drought-resistant or drought-tolerant landscaping shall be provided consistent with City of Ceres Water Efficient Guidelines and Standards.
- Development shall include low-impact development (LID) stormwater collection and treatment measures to control runoff flow and volume, such as vegetated swales, vegetated buffer strips, flow-through planters, bioretention areas, infiltration trenches, and other similar measures.
- If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.

### **Access**

- There shall be vehicular access from a public street or alley to off-street parking.
- All ingress and egress to and from public property shall be in a forward motion, with the exception of shared parking located on alleys.
- If a single development encompasses more than 40 percent of frontage on a single block, a publicly accessible pedestrian paseo leading to rear parking areas must be provided.

## D. Civic Center/Office District

The following standards apply to all development located in the Civic Center/Office District.



### Residential Density

- Maximum dwelling units per gross acre for residential development is 25.0.

### Floor Area Ratio (FAR)

- Maximum FAR for commercial and retail uses is 1.0.

### Height

- Buildings shall be no more than three stories.
- Ground floor commercial space shall be at least 14 feet and shall not exceed 20 feet, as measured from floor-to-floor. Upper stories shall not exceed 16 feet, as measured from floor-to-floor.

### Setbacks

- Front: None required, provided that no public utility easements conflict with the proposed development. Maximum front setback is 10 feet for all new buildings. Existing buildings converted from residential to commercial use are permitted at their existing setbacks.

- Side: None required.
- Exterior Side: None required.
- Rear Setback: 5 feet, except at blocks between Third Street and Fourth Street, which shall have a setback of 20 feet to alley right-of-way.

### **Parking**

- Multi-family Residential: 1 space per unit for 1-bedroom units and studios. 0.5 additional guest parking spaces per each additional bedroom.
- Single-family Residential: 1 space per unit.
- Commercial: 1 space for every 450 square feet of retail floor area.
- Office: 1 space for every 375 square feet of floor area, not including areas used expressly for storage.
- Medical office: See City of Ceres Zoning Code.
- Bicycle Parking: Bicycle parking spaces shall be provided at a rate equal to 5 percent of the total required parking spaces for commercial uses and 10 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).
- Parking Reduction: At blocks between Third Street and Fourth Street, reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an irrevocable access and public parking easement should be filed to ensure future owners cannot block access to shared parking areas.

### **Parking Lots**

- New parking areas are strongly encouraged to be located behind buildings and shall not be located within front setbacks or exterior side setbacks.

### **Landscaping**

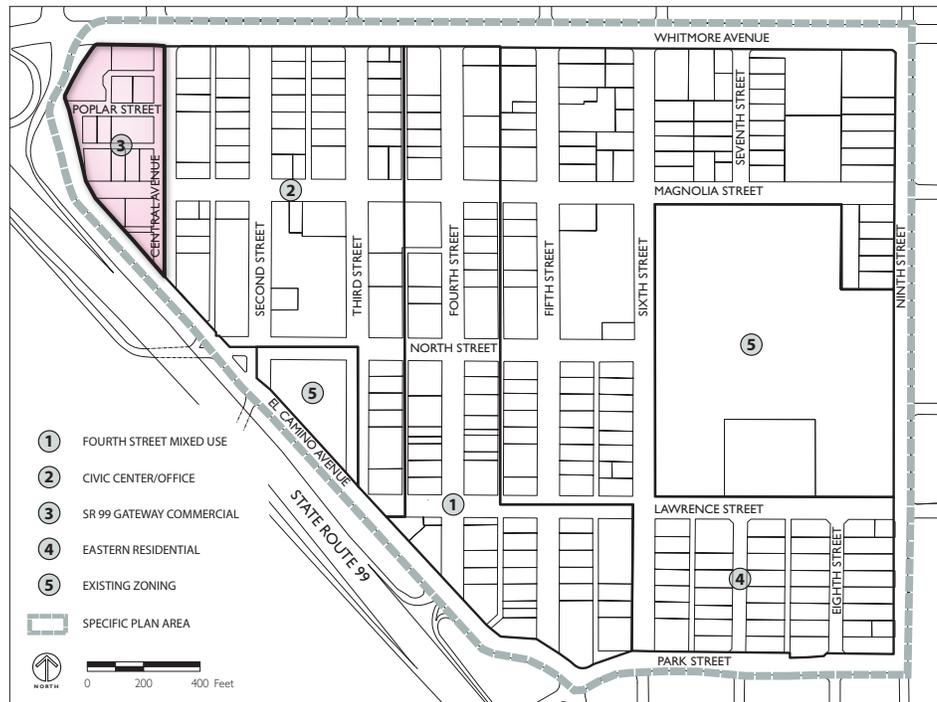
- Development shall maintain landscaping.
- Native and other drought-resistant or drought-tolerant landscaping shall be provided consistent with City of Ceres Water Efficient Guidelines and Standards.
- Development shall include low-impact development stormwater collection and treatment measures to control runoff flow and volume, such as vegetated swales, vegetated buffer strips, flow-through planters, bioretention areas, infiltration trenches, and other similar measures.
- If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.

### **Access**

- There shall be vehicular access from a dedicated street or alley to off-street parking.
- All ingress and egress to and from public property shall be in a forward motion.
- If a single development encompasses more than 40 percent of frontage on a single block, a publicly accessible pedestrian paseo leading to the rear parking areas must be provided.

## E. State Route 99 Gateway Commercial District

The following standards apply to all development located in the State Route 99 Gateway Commercial District.



### Floor Area Ratio (FAR)

- Maximum FAR for commercial and retail uses is 0.5.

### Height

- Buildings shall be no more than four stories.
- Ground floor commercial space shall be at least 14 feet and shall not exceed 20 feet, as measured from floor-to-floor. Upper stories shall not exceed 16 feet, as measured from floor-to-floor.

### Setbacks

- Front: None required, provided that no public utility easements conflict with the proposed development. Maximum front setback is 25 feet.
- Side: 10 feet.
- Exterior Side: None required.
- Rear Setback: 5 feet, or 50 feet from State Route 99, whichever is greater.

**Parking**

- Multi-family Residential: 1 space per unit for live-work units.
- Commercial: 1 space for every 450 square feet of retail floor area.
- Office: 1 space for every 375 square feet of floor area, not including areas used expressly for storage.
- Medical office: See City of Ceres Zoning Code.
- Bicycle Parking: Bicycle parking spaces shall be provided at a rate equal to 5 percent of the total required parking spaces for commercial uses and 10 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).

**Parking Lots**

- New parking areas are strongly encouraged to be located behind buildings and shall not be located within front setbacks or exterior side setbacks.

**Landscaping**

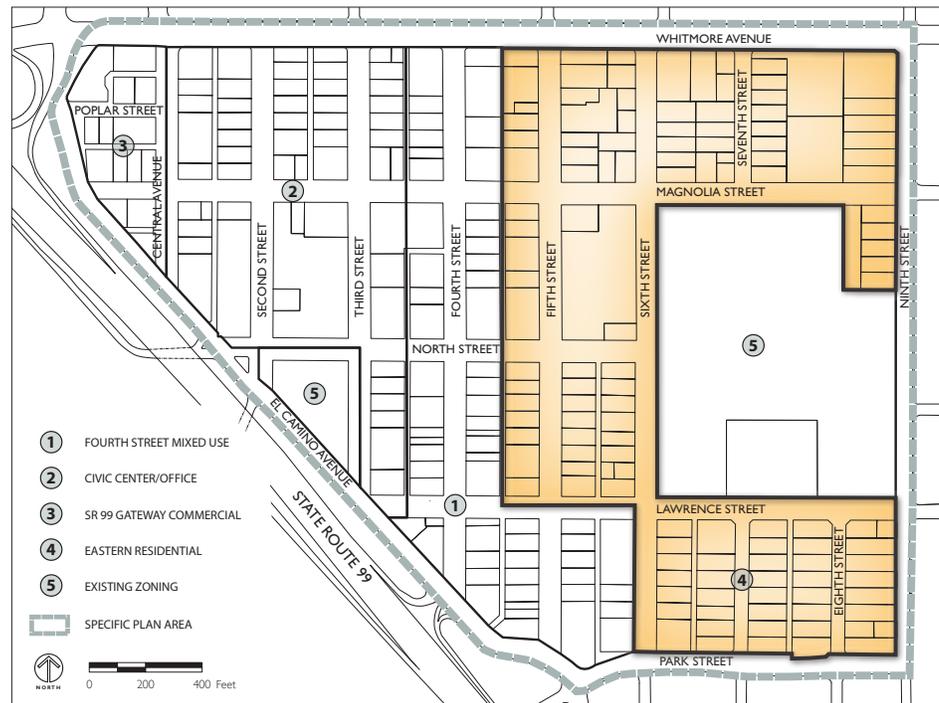
- Development shall maintain landscaping.
- Native and other drought-resistant or drought-tolerant landscaping shall be provided consistent with City of Ceres Water Efficient Guidelines and Standards.
- Development shall include low-impact development stormwater collection and treatment measures to control runoff flow and volume, such as vegetated swales, vegetated buffer strips, flow-through planters, bioretention areas, infiltration trenches, and other similar measures.
- If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.

**Access**

- There shall be vehicular access from a dedicated street or alley to off-street parking.
- All ingress and egress to and from public property shall be in a forward motion.
- If a single development encompasses more than 40 percent of frontage on a single block, a publicly accessible pedestrian paseo leading to the rear parking areas must be provided.

## F. Eastern Residential District

The following standards apply to all development located in the Eastern Residential District.



### **Residential Density**

- Maximum dwelling units per gross acre for residential development is 30.0.

### **Floor Area Ratio (FAR)**

- Maximum FAR for commercial and retail uses is 1.0.

### **Height**

- Buildings shall be no more three stories.
- Stories shall not exceed 14 feet, as measured from floor-to-floor.

### **Setbacks**

- Front: 10 feet.
- Side: 5 feet for existing and new single-family residential units or existing multi-family units. None required for new multi-family residential that can meet fire code requirements of the California Building Code.
- Exterior Side: 10 feet.
- Rear Setback: 5 feet, except at blocks between Fifth Street and Fourth Street,

which shall have a setback of 20 feet to alley right-of-way.

### **Parking**

- Multi-family Residential: 1 space per unit for 1-bedroom units and studios. 0.5 additional guest parking spaces per each additional bedroom.
- Single-family Residential: 1 space per unit.
- Commercial: 1 space for every 450 square feet of retail floor area.
- Office: 1 space for every 375 square feet of floor area, not including areas used expressly for storage.
- Bicycle Parking: Bicycle parking spaces shall be provided at a rate equal to 5 percent of the total required parking spaces for commercial uses and 10 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).
- Parking Reduction: At blocks between Fifth Street and Fourth Street, reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys.

### **Parking Lots**

- New parking areas are strongly encouraged to be located behind buildings and shall not be located within front setbacks or exterior side setbacks.

### **Landscaping**

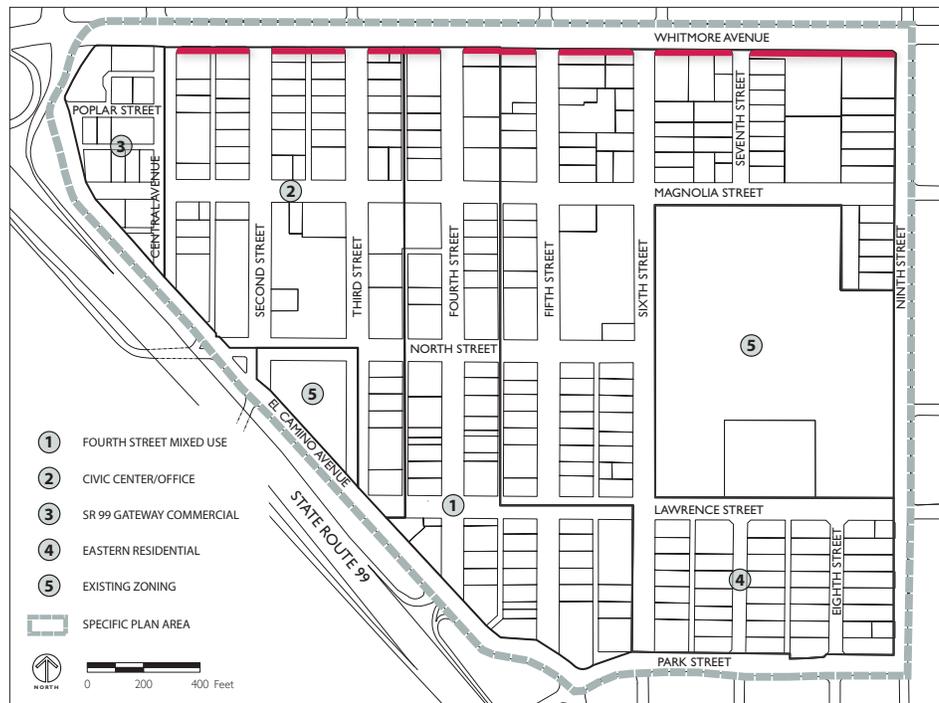
- Development shall maintain landscaping.
- Native and other drought-resistant or drought-tolerant landscaping shall be provided consistent with City of Ceres Water Efficient Guidelines and Standards.
- Development shall include low-impact development stormwater collection and treatment measures to control runoff flow and volume, such as vegetated swales, vegetated buffer strips, flow-through planters, bioretention areas, infiltration trenches, and other similar measures.
- If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.

### **Access**

- There shall be vehicular access from a dedicated street or alley to off-street parking.
- All ingress and egress to and from public property shall be in a forward motion.
- If a single development encompasses more than 40 percent of frontage on a single block between Fourth Street and Fifth Street, facing Fifth Street, a publicly accessible pedestrian paseo leading to the rear parking areas must be provided.

## G. Whitmore Avenue Frontage Standards

The Whitmore Avenue Overlay is intended to ensure that new development adjacent to Whitmore Avenue relates appropriately to this major arterial and that setbacks are increased to allow for landscaping and improved sidewalks, as well as to support a boulevard condition. The following development standards augment standards provided in preceding sections for each district for development fronting onto Whitmore Avenue. For standards not listed in this section, development shall comply with the underlying district standards.



### Floor Area Ratio (FAR)

- Maximum FAR for commercial and retail uses is 3.0.

### Height

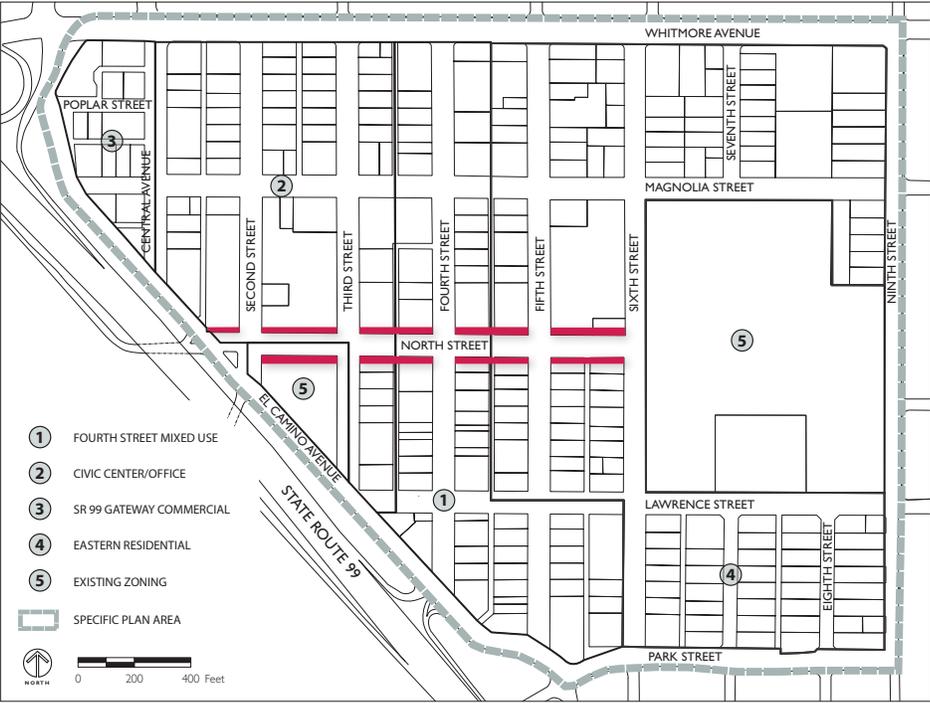
- Buildings facing Whitmore Avenue shall be no more than three stories.

### Setbacks

- Buildings shall be set back a minimum of 10 feet along the Whitmore Avenue frontage.

**H. North Street Frontage Standards**

The North Street Overlay provides special development standards to ensure that new development is set back appropriately from North Street. Standards support the vision for North Street of a Green Connection. The “green street” is intended to connect open spaces through Downtown, utilize sustainable stormwater management technologies and provide lush landscaping. The North Street Green Connection concept is described in detail in Chapter Four. These development standards augment standards provided in preceding sections for each district for development abutting onto North Street. For standards not listed in this section, development shall comply with the underlying district standards.



**Setbacks**

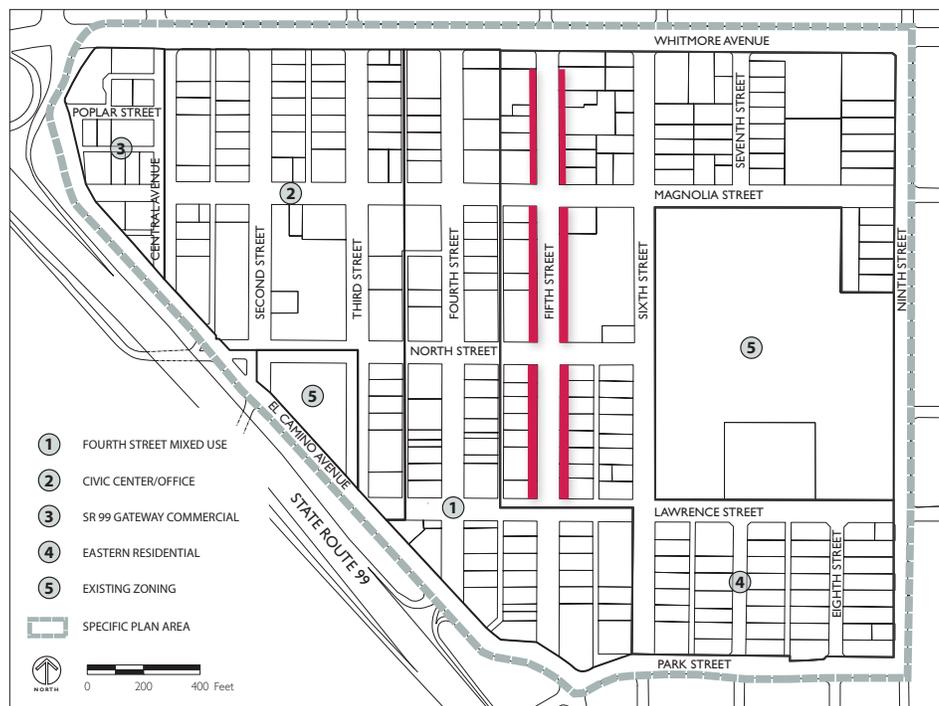
- Buildings shall be set back a minimum of 10 feet from North Street

**Landscaping**

- Landscaping treatment shall be provided within setbacks adjacent to North Street.

## I. Historic Fifth Street Frontage Standards

The Fifth Street Overlay provides special development standards to ensure that new development respects the existing character and development along Fifth Street from Lawrence Avenue to Whitmore Avenue. These development standards augment standards provided in preceding sections for the Eastern Residential District for development that fronts onto Fifth Street. For standards not listed in this section, development shall comply with the underlying district standards.



### Height

- The portion of buildings within 20 feet of the property line abutting Fifth Street shall be no more than two stories.

### Setbacks

- Buildings shall be set back an average of 20 feet from Fifth Street. Setbacks for individual building elements are encouraged to vary from 10 to 30 feet.

### Landscaping

- Development that is adjacent to Fifth Street, but does not have its primary frontage on Fifth Street, shall provide significant landscaping or a publicly accessible open space between the building edge and Fifth Street.

***Additional Historic Standards on Fifth Street***

In order to preserve the visual character of historic resources along Fifth Street, the following standards shall apply to development on parcels abutting historic resources.

- Subject properties: Parcels abutting Whitmore Mansion (APN # 127015017) and the Whitmore Home and Museum (APN # 127017035).
- Setbacks: Parcels to the side and rear of a historic structure shall be set back a minimum of 20 feet from the lot line abutting the historic structure's lot line. No parking shall be allowed in this setback and it shall include landscaping appropriate to the historic character of adjacent properties.
- Height: Within 30 feet from the lot line abutting the historic structure's lot line, height of new development shall be no greater than the height of the abutting historic structure.
- Landscape buffer: Within 30 feet from the lot line abutting the historic structure's lot line, height of new development shall be no greater than the height of the abutting historic structure.

## **J. Definitions**

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***Building Height***

Height of a building given in number of habitable stories above grade.

***Commercial Recreation, Indoor***

Any establishment that provides entertainment activities or services for a fee or admission charge, including bowling alleys, electronic game arcades, billiard rooms, sports clubs, and other uses of like kind or character. An establishment that offers a small number of game machines to its customers as an accessory use, such as a restaurant or laundromat, shall not be considered a commercial recreational facility. Facilities operated by a public agency are not included in this definition.

***Day Care Facility***

Any facility that provides non-medical care and supervision of minor children for periods of less than 24 hours; that does not qualify as a small or large family day care home; and that meets the licensing requirements of the State. Any establishment may provide child day care as an accessory use that is not subject to additional permit requirements, provided that the establishment offers child day care only to its customers or employees, and only during the period when the customers or employees are visiting or working in the establishment.

***Emergency Shelter***

A structure that provides temporary shelter and feeding of homeless or disaster victims, and that is operated by a public or non-profit agency.

***Exterior Side Setback***

A side setback abutting a street.

***Floor Area***

The sum of the gross horizontal areas of the several floors of the building, excluding areas used for accessory garage purposes and such basement and cellar areas as are devoted exclusively to uses accessory to the operation of the building. All horizontal dimensions shall be taken from the exterior faces of walls including walls or other enclosures of enclosed porches. Whenever the term “floor area” is used in this Specific Plan as a basis for requiring off-street parking for any structure, it shall be assumed that, unless otherwise stated, floor area applies not only to the ground floor area but also to any additional stories and/or basement of the structure.

***Live/Work Units***

Buildings or spaces within buildings that are used jointly for commercial and residential purposes where the residential use of the space is secondary or accessory to the primary use as a place of work.

***Meeting Facility***

A facility for public or private meetings or events, not including sports or other commercial entertainment facilities, convention centers or community centers. Includes clubs and private meeting halls, museums or other facilities intended to host events, such as weddings, graduation parties, quinceñeras, receptions or other similar events.

***Overlay***

An area or street frontage in which additional standards apply beyond the base district standards.

***Personal Services***

An establishment other than a professional office that provides services to individuals as a primary use, and that may provide accessory retail sales of products related to the services provided. Typical uses include, but are not limited to beauty and barber shops, nail salons, spas, watch and jewelry repair, shoe repair shops and tailor shops.

***Public Safety Facility***

A facility operated by a public agency for the purpose of protecting public safety, including but not limited to fire stations and other fire-fighting facilities, police stations, and ambulance dispatch facilities.

***Restaurant***

Any retail business that sells ready-to-eat food or beverages for on-premise or off-premise consumption, excluding “food and beverage sales” uses.

***Retail, General***

A commercial enterprise that provides goods and/or services directly to the consumer where such goods are available for immediate purchase and removal from the premises by the purchaser. Does not include any specific retail uses that are listed separately.

***Retail, Neighborhood***

A commercial enterprise that provides goods and/or services directly to the consumer where such goods are available for immediate purchase and removal from the premises by the purchaser, but are do not have a total floor area greater than 5,000 square feet. The stores in this Zone are intended to fit into the residential pattern of the surrounding neighborhood without creating either architectural or traffic conflicts. Does not include any specific retail uses that are listed separately.

***Setback***

The line which defines the depth of the required yard. The setback line is parallel with the street or property line, or when established by general plan, with the highway right of way, measured to the perpendicular distance given.

***Social Assistance Services***

Facilities other than offices providing a social service directly to the adjacent community, such as food banks, blood blanks, crisis centers or similar.

## 7 LAND USE REGULATIONS & DEVELOPMENT STANDARDS

## CIRCULATION 8

This Specific Plan proposes a number of circulation improvements. Recommendations focus on sustainable strategies that accommodate vehicles, pedestrians, bicycles and transit.

## A. Pedestrian Improvements



Sidewalk on a residential street in Downtown.

### Sidewalk System

Pedestrian circulation is a major priority for Downtown. As an early step in the implementation process, it is proposed that an integrated pedestrian circulation system be developed for Downtown. An initial step in establishing a more robust pedestrian system is the completion of the existing sidewalk system, particularly in the eastern residential areas. Priority sidewalk segments include areas of Fifth Street near the Whitmore Mansion where there are currently no sidewalks. Smaller gaps should also be closed along Magnolia Avenue, Fifth Street near Whitmore, and potentially along El Camino Avenue. Figure 2-1 in Chapter Two, Existing Conditions, shows gaps in the existing Downtown sidewalk system.

### Paseo Network

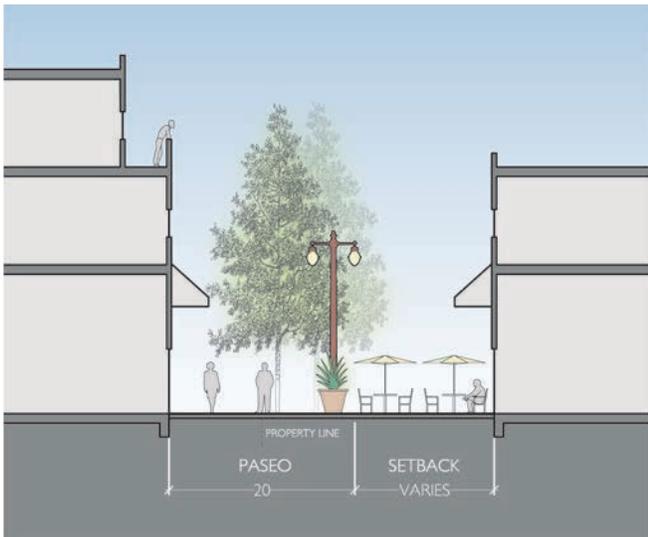
A key component of the Specific Plan is increased east-west pedestrian circulation. To that end, a series of pedestrian paseos is proposed for the Plan Area. It is envisioned that one of these connections would cross the blocks bounded on the north and south by Magnolia Street and North Street respectively. A second pedestrian paseo is proposed to cross the blocks bounded on the north and south by North Street and Lawrence Street respectively. The proposed paseo network will connect key destinations in the Plan Area to Fourth Street and to each other, including the Whitmore Home, Whitmore Mansion and Whitmore Park. The paseo network will be instrumental in the establishment of a walkable downtown in Ceres. Furthermore,



Paseo in Stockton.

a paseo system will provide enhanced character and identity to Downtown and promote economic development by making nearby sites more attractive. Paseos will bisect blocks containing parcels under private ownership and will potentially require property acquisition by the City, dedication, public/private partnerships or other mechanisms. In addition to their contribution to a comprehensive pedestrian circulation system, paseos will also provide increased visual interest in the Downtown and evolve into unique public gathering spaces. Development adjacent to paseos can “spill out” onto the paseo with outdoor seating, special landscaping and other similar physical features. Paseos could accommodate special events, such as sidewalk sales, farmers’ markets and other appropriate activities. Paseos should also be comfortable pedestrian spaces at night and should be available for nighttime events. Adequate lighting and engagement of paseos by new development and residential units on upper floors of mixed use buildings will help to ensure this. Figures 8-1 and 8-2 illustrate a typical paseo during the day and at night.

**FIGURE 8-1** Paseo



**FIGURE 8-2** Paseo at Night



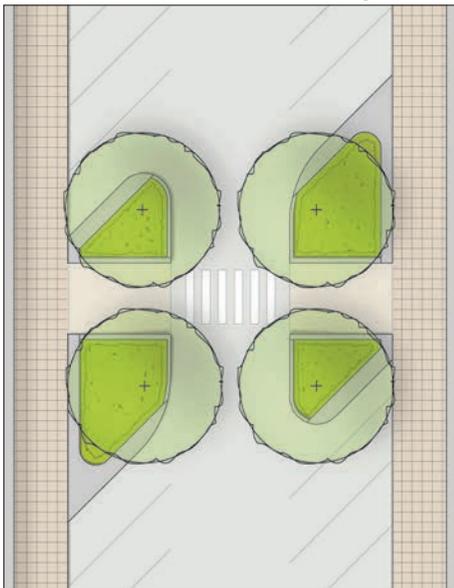
### Pedestrian Signalization

New traffic signal improvements proposed for Downtown are discussed in detail below. In addition to easing vehicular traffic, new traffic signals should provide protected pedestrian crossings as appropriate and accommodate bicyclists, ultimately providing easier and safer access to Downtown for these modes. Signalized pedestrian crossings will be most appropriate in later implementation stages, as traffic increases.

**Pedestrian Crossings**

Well-marked pedestrian crossings provide enhanced safety and comfort for pedestrians and help calm traffic. While several pedestrian crossings already exist in Downtown, including marked along Whitmore Avenue and near Walter White Elementary School, additional clearly-marked pedestrian crossings should be provided in Downtown. Marked crossings should be incorporated at all Downtown intersections and at key mid-block crossings. Mid-block crossings are proposed along Third and Fourth Street. These are proposed to connect paseos across streets in the middle of blocks. Additional mid-block crossings should also be considered in other locations as Downtown redevelops. Mid-block crossings enhance the pedestrian circulation system, increase pedestrian safety, enhance streetscape character, and can help to break up the monotony of long blocks in Downtown. Figure 8-3 illustrates how a mid-block crossing could be incorporated on Fourth Street. As shown, some parking spaces will need to be removed to accommodate a new crossing.

**FIGURE 8-3** Mid-Block Crossing



Crosswalk.

## B. Vehicular Improvements

### El Camino Avenue/North Street/State Route 99 Ramps Improvements

This important intersection is a gateway into Downtown Ceres. Under its current configuration, the intersection is not able to accommodate an increase in traffic associated with regional growth and the development of the Specific Plan land uses. A traffic signal will need to be installed at the El Camino Avenue/North Street/State Route 99 ramps to better facilitate traffic flows. The Specific Plan recommends a reconfiguration of the intersection that would close a small segment of Second Street between El Camino Avenue and North Street. Closure of this short segment that creates the existing triangular island would simplify signalization, and allow for expansion of Whitmore Park. The recommended intersection configuration is shown on Figure 8-4. Since this intersection was built prior to the current Caltrans standards being in place, a design exception may be required to make improvements. This improvement is not warranted in the near-term, but rather to accommodate long-term Specific Plan Area and regional growth. It is estimated that the design of the improvements should begin when building permits representing 35 percent of Specific Plan buildout are issued. It is recommended that construction be completed before the Specific Plan Area reaches 75 percent buildout. More information about transportation improvement costs and potential traffic impact fees are provided in Chapter Ten, Implementation.

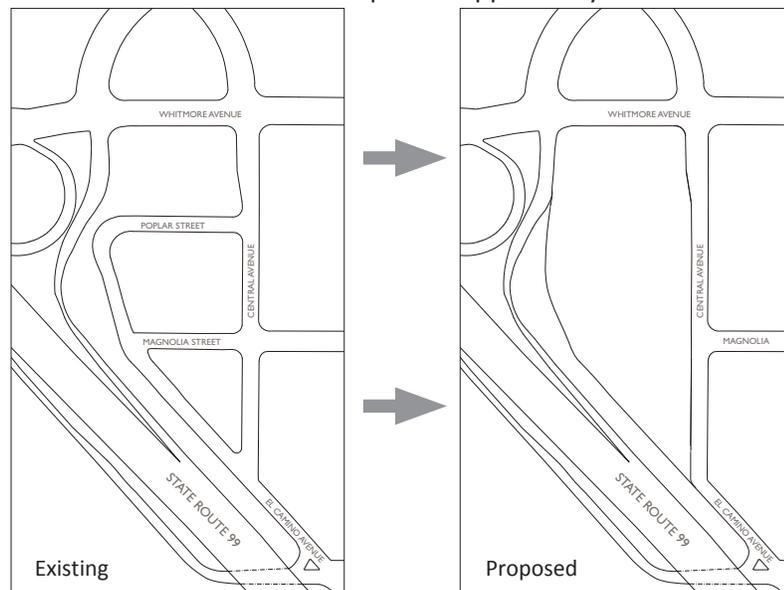
**FIGURE 8-4** Ramp Improvements



### El Camino Avenue and Magnolia Street Elimination West of Central Avenue

The State Route 99 Whitmore Interchange project, at Whitmore Avenue, will end El Camino Avenue prior to reaching Whitmore, and direct northbound El Camino Avenue traffic onto Magnolia Street. The plan proposes to dedicate the right-of-way of these streets to create a large redevelopment parcel at the northwest portion of the Downtown, provided that surrounding parcels can be consolidated. The assembly of these blocks provides a noteworthy development opportunity for the Downtown. Redevelopment of this area would take place in late phases of implementation, and careful coordination with existing property owners will be required. Figure 8-5 shows how these blocks could potentially be transformed to create a large, highway-oriented development opportunity.

**FIGURE 8-5 Northwest Development Opportunity**



### Lane Width Reduction on Downtown Streets

Many streets in Downtown have wide right-of-ways with ample room for travel and parking. Several streets have a curb-to-curb distance of 60 feet. Reducing travel lanes to a width of ten to eleven feet will provide opportunities for enhancement of the pedestrian realm, landscape and streetscape improvements, reconfigured parking, bicycle facilities, and increased development potential. Travel lanes of ten to twelve feet on Downtown streets can still provide for safe travel and emergency access while helping to satisfy urban design goals. Travel lane reduction will also help minimize

paved surfaces in Downtown that impact stormwater infrastructure facilities. Lane width reductions should occur as part of larger streetscape projects in Downtown. Additional detail and illustrations regarding the reconfiguration of streets in Downtown are provided in Chapter Four, Development Concepts.

### **Creative Streetscape Improvements at Strategic Locations on Fifth Street**

The Specific Plan prioritizes the preservation and enhancement of historic Fifth Street. Creative streetscape improvements will be critical to the street’s transformation to a celebrated historic corridor. It is proposed that new streetscape configurations be explored as parcels adjacent to this street redevelop. For example, it may be appropriate for Fifth Street to “shift” at certain locations or intersections where opportunities to improve the right-of-way exist, particularly where improvements will accentuate properties or features to reinforce the historic character of the street. Opportunities include the west side of the mansion property, which has been identified by this Specific Plan as a key termination point of the northern paseo.



Meandering road provides interest and character to the street.

### **North Street/Fourth Street Traffic Circle Removal**

It is proposed that the existing traffic circle at the North Street/Fourth Street intersection be removed. Removal of this traffic circle will allow for more extensive pedestrian improvements, including enhanced sidewalk bulbouts at this primary intersection. This action will also create design continuity on Fourth Street and provide for easier mobility and vehicle access. A four-way stop will need to be added at this intersection upon removal of the traffic circle to ensure safe traffic operations. Please refer to Chapter Four, Development Concepts, for further discussion of potential improvements to this intersection and North Street as a whole.

### **Pine Street/El Camino Avenue Intersection**

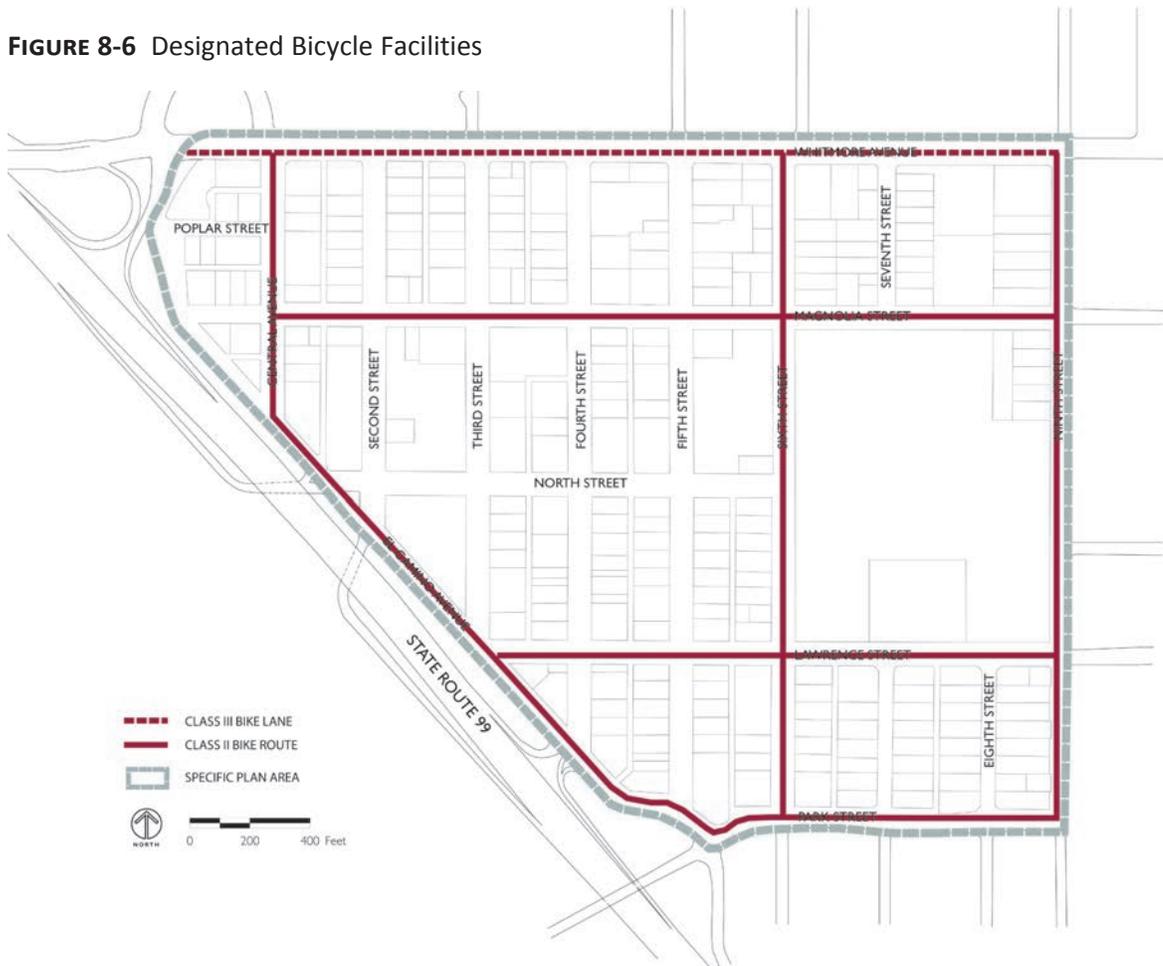
This intersection was examined during the planning process to determine if improvements or signalization would be warranted. It was determined that this intersection is currently operating at an acceptable level and will continue to operate at an acceptable level upon buildout of the Specific Plan. Additionally, due to the close spacing of this intersection to the State Route 99 over crossing, sight distance is limited over the bridge structure and there is insufficient distance between the crest of the bridge and expected eastbound vehicle queues (should the intersection meet signal warrants and be signalized) to provide sufficient stopping distance.

## C. Bicycle Improvements

### Class III Bicycle Facilities

A Class III bicycle route is a street or section of street that is designated as a shared route for bicycles and vehicles. Signage or markings on the street alert vehicular traffic to bicycle traffic. Magnolia Street contains the only Class III bicycle route already in place in Downtown. New Class III routes are recommended for El Camino Avenue, Park Street (from Pine Street to Sixth Street), Lawrence Street, Ninth Street, Sixth Street and Central Avenue. These improvements are consistent with recommendations in the Ceres General Plan, which recommends Class III or Class II bike lanes (dedicated on-street bike lane) for these roadways. Designated bicycle facilities are shown in Figure 8-6.

FIGURE 8-6 Designated Bicycle Facilities



### Class II Bicycle Improvements

Whitmore Avenue is also designated in the Ceres General Plan as a Class II or Class III facility. It is recommended that this roadway be reconfigured to include an on-street Class II bike lane. A Class II bike lane is a dedicated area within the roadway, which is demarcated by striping or painting and is available exclusively to bicyclists. Bike lanes are appropriate for Whitmore Avenue, particularly considering its close proximity to Downtown, the crossing of State Route 99 and two major nearby school facilities.



Class II Bicycle Lane.



Class III Bicycle Route.

### Bicycle Parking

It is recommended that bicycle parking facilities be provided as development occurs. Bicycle parking facilities should be provided as areas redevelop and increased bicycle activity emerges in Downtown. The City should provide bicycle parking at community parks and buildings wherever possible, and potentially along Fourth Street as well. Standards for bicycle parking in private development are provided in Chapter 7, Land Use Regulations and Development Standards.

## D. Parking Improvements

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### Fourth Street Angled Parking

The southern portion of Fourth Street from North Street to El Camino Avenue is currently lined on both sides with angled parking. This is also where Fourth Street exhibits the commercial character of a true downtown. Building on this character, it is proposed that the entire street frontage along Fourth Street be converted to angled parking to create a consistent streetscape within this important commercial corridor, as well as to provide additional on-street parking. Stormwater treatment measures should be engineered with properly placed flow through curbs or curb cuts to allow for positive drainage. These and similar issues will need to be studied more closely on a street-by-street basis by a landscape architect and engineer during future streetscape design processes.



Fourth Street Angled Parking, just north of El Camino Avenue.

### **Fourth Street Alley-accessed Parking**

The Specific Plan envisions a continuous reservoir of off-street parking along the north-south alleys flanking Fourth Street. This parking would be provided through a variety of mechanisms. Development along these alleys is required to be set back from the alley right-of-way by 20 feet, with parking provided in that setback. Private developers are allowed to provide a smaller number of parking spaces if the spaces are made publicly accessible. Additionally, the City can acquire the land or dedications along the alleys to create public parking.

### **South Fourth Street Parking Lot**

The Specific Plan recommends development of a new parking lot south of the new Turlock Irrigation District building via property acquisition and coordination with the existing TID building. New public surface parking should be combined with the TID office's customer parking area located adjacent to El Camino Avenue. This site will provide additional parking resources for Fourth Street businesses and the Fourth Street Cinema on days experiencing peak demand. Retail development along Fourth Street will be able to rely on this public lot in part to satisfy parking needs. This improvement would be appropriate in late stages of implementation as parking demand increases in response to new development. Please see Figures 4-7 through 4-10 in Chapter Four, Development Concepts, for an illustration of how a new parking area could be developed at this location.

### **North Street Parallel Parking**

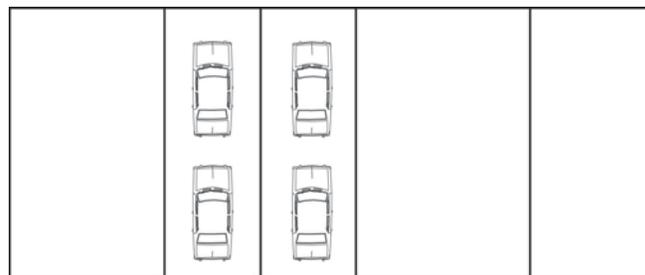
North Street is currently configured with a mix of parallel and angled parking in Downtown. This Specific Plan proposes improvements to the existing right-of-way configuration on North Street. Chapter Four, Development Concepts, discusses the proposed streetscape improvements in detail. The North Street Green Connection discussed in Chapter Four recommends that North Street be reconfigured to include deeper setbacks, a rich tree canopy and bioswales to manage stormwater. Converting existing angled parking to parallel parking along North Street will provide space to expand the pedestrian realm as well as provide bioswales, which reinforce sustainability goals for Downtown. It is recommended that improvements to North Street be undertaken in the early phases of development, as a part of a more detailed design process.

### Civic Center Parking

Chapter Three, Vision, Goals and Concept, recommends that a new Civic Center parking area be provided on the north side of the block bordered by Second and Third Streets, where the existing library and City Hall are now located. Stanislaus County has already designated Ceres as the location for a new 25,000 square foot library. The Specific Plan strongly recommends that this new library be built in Downtown. Upon construction of this facility, it is anticipated that the current library can be acquired by the City and converted to surface parking, to serve the Community Center, Fire Station #1, Police Building and the new City Hall recommended south of the Community Center. These improvements can be phased, with the parking area expanding as the existing City Hall and Library are replaced. This parking area will also be linked directly to the northern paseo system. Civic Center parking is intended to provide long-term parking for Civic Center uses during the day and at night, but could also create shared parking opportunities for other Downtown uses at night, such as restaurants, shopping, and nighttime community events. Parking capacity could be further increased in the future with the provision of a parking structure.

### Tandem Parking in Private Development

It is recommended that the City consider allowing tandem parking to be provided in future private development in Downtown. This type of parking, illustrated in the drawing below, can sometimes provide for more efficient parking layout. Any tandem parking provided in private development should be in keeping with City standards.

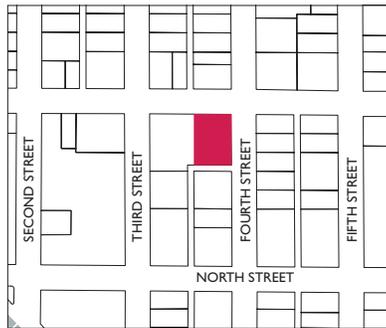


## **PUBLIC FACILITIES, SERVICES & INFRASTRUCTURE** 9

This chapter provides recommendations for improvements to public facilities, services and infrastructure.

## A. Parks and Open Space

**FIGURE 9-1** Plaza Location

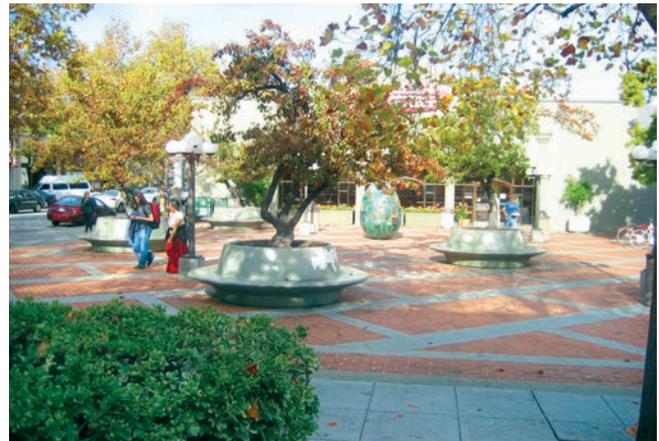


### Civic Plaza

A new civic plaza is proposed on the existing Community Center parking lot, the location of which is shown Figure 9-1. This concept is detailed in Chapter Three of this Specific Plan. This plaza will provide a new amenity on Fourth Street and will create a new central public space for the entire Downtown. The civic plaza will also be suited to hold events, such as farmers’ markets and other similar activities. This improvement should be phased with a comprehensive expansion of the existing Civic Center, which would include a strategy to provide replacement parking for the parking spaces currently located at this site.



Downtown plaza.



Downtown plaza.



Whitmore Park.

### Whitmore Park Improvements

New landscape improvements are proposed for Whitmore Park, including a landscaped enclosure at the western edge to provide increased enclosure for the park, provide a physical buffer between the park and the freeway, and help mitigate freeway noise. A new landscape enclosure and other park improvements should be designed according to Crime Prevention Through Environmental Design (CPTED) and should allow for visibility into Whitmore Park from surrounding roadways. CPTED principles are discussed in greater detail in Section B of Appendix A of this Specific Plan. Improvements should be designed to preserve and enhance existing features, such as the barbecue area, war memorial, and gazebo. It is recommended that improvements to Whitmore Park be undertaken by the City in the middle phases of the fifteen year implementation schedule for the Specific Plan, which is detailed in Chapter Ten, Implementation. Cost estimates and potential financing strategies are also provided in Chapter Ten.

### **Whitmore Green**

A new green space is proposed for the Whitmore Mansion site. This new green space will complement the existing Whitmore Park and provide an important new amenity for this portion of the Downtown. Provision of this space may require a public private partnership to ensure public access. It is proposed that the Whitmore Mansion continue to function as a special event center for Ceres. As Downtown redevelops and parking demand for functions at the Mansion increase, shared parking opportunities should be explored with the school properties across the street. Parking facilities for School District properties are generally unused in the evening.

### **Water Tower Pocket Park**

A new small park is proposed along Fifth Street, adjacent to the historic Whitmore House and Museum and the Ceres Water Tower. This park would provide a neighborhood amenity and celebrate these historic structures. It would also act as a pedestrian and bicycle connector between Fifth Street and Sixth Street. A new park at this location should be designed according to Crime Prevention Through Environmental Design (CPTED) principles, which are discussed in greater detail in Section B of Appendix A in this Specific Plan.



Neighborhood Park.



Gondring Memorial Library.

## B. Libraries

The Gondring Library is located at the southwest corner of the Third Street/Magnolia Street intersection. This existing library is approximately 4,500 square feet. A library is a healthy civic use for Downtown and it is recommended that the existing library be replaced with a new facility with increased capacity.

Stanislaus County's 2006-2007 Capital Improvement Plan identifies Ceres as the future location of a 25,000-square foot regional library facility. Building on this, a new library is recommended for the west side of Second Street, across from the existing courthouse facility. A new library at this location will become the western bookend of an expanded Civic Core and will provide a meaningful terminus for the pedestrian paseo.

## C. Public Services

Many of the public safety facilities are located in the Downtown on the west side of Third Street, between Magnolia and North Streets.



Fire Station #1.

### Fire Service

Fire Station #1, the City's recently constructed Downtown station, has adequate capacity to serve the needs of development anticipated by the Specific Plan. Fire Station #1, located within the Downtown, would serve new development associated with the Plan, with Station #4 providing backup if needed. Emergency service is a critical advantage in that these services would be quickly available to increased resident numbers anticipated to occur with implementation of this Specific Plan. Given this location, new development within Downtown would likely be served within the average response time, or less. New residents and commercial, office, and civic space recommended in the Specific Plan would require the hiring of additional personnel. However, the additional firefighters could be accommodated within Station #1 and the other stations, which are not fully staffed. This fire station will remain in place through implementation, and any expansion of the existing Civic Center should take into account and be closely coordinated with fire service operations.



Ceres Police Building.

### Police

The Police Division's headquarters is located within the Public Safety Building on Third Street in Downtown and includes Administrative Offices, the Investigative Bureau, the Patrol Bureau, the Record Bureau, the Volunteers in Public Service Training, Community Service Officers, the Central Dispatch Office, a parking facility, and a storage area for equipment. This provides an extremely important resource for Downtown

as it redevelops and increases in density, and demand increases for police services. This valuable Downtown resource should remain in place through implementation, and any expansion of the existing Civic Center should take into account and be closely coordinated with police operations.

## D. Educational Facilities

This section discusses the future of education facilities in and near Downtown.

### Downtown School District Facilities

Walter White Elementary, Argus Continuation/Endeavor Alternative High School and a number of school district administration buildings are located in Downtown. The school properties are generally bounded by Sixth and Ninth Streets at the west and east, respectively. Magnolia and Lawrence Streets form the northern and southern boundaries. It is proposed that these academic uses continue to function as schools and administration buildings. City staff should work with the school district to identify opportunities for student activities and to ensure that any property improvements are consistent with the vision for Downtown. If school facilities are relocated in the future, City staff should work with the school district on property conveyance. If redeveloped, school properties could present an array of development opportunities for Downtown.

### Ceres High School

Ceres High School is located just north of Downtown, across Whitmore Avenue. Although Ceres High School does not serve students living in the Specific Plan Area, the school is a key institutional use in Downtown and its impact should be considered as new development and public improvements take place. As such, improvements to Whitmore Avenue in Downtown should be coordinated with the School District, to ensure safety and seamless integration with student transportation services. City staff should work with Ceres High School to identify opportunities for student activities and property improvements which are consistent with the vision for Downtown.



Ceres High School.

### Future School Capacity

Build out of the Specific Plan will add students to schools that already exceed capacity. To mitigate this, developers for projects within the Downtown will be required to pay school impact fees.

## E. Utility Infrastructure

This section provides an overview of the utility infrastructure improvements that will be required to support the development anticipated from the Specific Plan. Recommendations for storm drainage, wastewater and water utilities are provided. Additional details regarding existing infrastructure and improvements to accommodate Specific Plan implementation can be found in the Downtown Ceres Specific Plan/EIR Infrastructure Summary Memo prepared by Jacobs Engineering Group in 2009. Chapter Ten, Implementation, provides details about the cost of all proposed infrastructure improvements described below. Since utility infrastructure issues are often a citywide issue, Downtown-specific infrastructure improvements should be re-evaluated as system improvements are made in other parts of the city. All new development in Downtown will pay impact fees toward funding required infrastructure improvements.

### Water

Domestic water is provided to Downtown by the City of Ceres municipal water system. Wells are Ceres’ only water supply source.

#### Water System Assessment

The existing water system was examined and the following assessments were made:

- The City of Ceres water system has enough volume to serve the Downtown Specific Plan study area in average day and maximum day demands.
- The City of Ceres water system does not have sufficient pressure to serve the Downtown Specific Plan Area under fire flow demands.
- In order to increase pressure in the study area, the City should install additional pumps, renovate the existing water tower or construct a new water tower.
- Upgrades to a number of pipes will be required, and two options are presented in the study mentioned above. It is recommended that the City implement Option 2, upgrade pipes in Magnolia Street, since that will result in easier construction and fewer direct connections to be replaced.

**TABLE 9-1** Specific Plan Area Water Demand

	Average (gpm)	Peak (gpm)
Existing	144	440
Proposed	178	540

Table 9-1 shows existing water demand in the Downtown and projected demand under implementation of the Specific Plan in gallons per minute (gpm).

**Recommended Water System Improvements**

Two options are presented for upgrades to the existing water infrastructure system to ensure adequate capacity under implementation of the Specific Plan. Option 1 includes removing the existing 6-inch water pipes within the Fourth Street/Fifth Street Alley, North Street, Second Street, and Magnolia Street, and replacing them with an 8-inch main connection to the existing 10-inch main in Lawrence Street. The Central Avenue pipe would be replaced with a new 12-inch main. Table 9-2 shows the improvements recommended in Option 1. Costs to implement Option 1 are contained in the Downtown Ceres Specific Plan/EIR Infrastructure Summary Memo prepared by Jacobs Engineering Group in 2009.

**TABLE 9-2** Recommended Improvements to Water System (Option 1)

Pipe Location	Improvement Limits	Length of Pipe (ft)	Existing Diameter (in)	New Diameter (in)
Fourth / Fifth Street Alley	Lawrence Street to North Street	515	6	8
North Street	Fourth / Fifth Street Alley to Second Street	500	6	8
Second Street	North Street to Magnolia Street	600	6	8
Magnolia Street	Second Street to Central Avenue	350	6	8
Central Avenue	Magnolia Street to Whitmore Avenue	550	6	12

A second option for water system improvement is to remove the existing 6-inch water pipes within Magnolia Street and Central Avenue and replace them with 8-inch and 12-inch connections respectively to the existing 8-inch main in Ninth Street. Table 9-3 shows the improvements recommended in Option 2. Costs to implement Option 2 are contained in the Downtown Ceres Specific Plan/EIR Infrastructure Summary Memo prepared by Jacobs Engineering Group in 2009.

**TABLE 9-3** Recommended Improvements to Water System (Option 2)

Pipe Location	Improvement Limits	Length of Pipe (ft)	Existing Diameter (in)	New Diameter (in)
Magnolia Street	Ninth Street to Central Avenue	2,630	6	8
Central Avenue	Magnolia Street to Whitmore Avenue	550	6	12

Although Option 1 would have lower materials costs, improvements would traverse an alley. It is assumed that Option 1 will require more reconnections to existing services. In addition, there are sewer and dry utility connections within a number of alleys in the Downtown. Replacement of the water pipe within the Fourth Street/Fifth Street Alley may become cumbersome once the existing utilities and direct service connections are encountered. As stated above and for these reasons, Option 2 is recommended for Downtown.

Regardless of which improvement option is implemented, a new water tank or potable well may need to be installed to accommodate demand for increased water pressure under implementation of the Specific Plan. The citywide water system should be carefully considered upon design of a new water tank. It is proposed that the existing water tower be retained for the character and aesthetic value it adds to Downtown.

The estimated cost to implement Option 2 improvements to support implementation of the Specific Plan is presented in Chapter 10, Implementation.

### **Stormwater**

Storm drainage in Downtown is managed via overland flow in gutters, an underground storm drainage pipe system, several dry well systems, and various french drain systems. Storm drainage system capacity is currently inadequate to convey the 10- and 100-year storm events. Furthermore, a French drain was recently installed on Ninth Street, which makes this area one of the most well-drained areas in Ceres.

#### ***Stormwater Infrastructure Assessment***

After review of the existing and proposed conditions, the following issues have been identified and will be further discussed in this section:

- Where a storm drain system exists, pipes should be upgraded to carry the 10-year (10 percent chance of recurrence) storm.
- Downtown areas currently served by existing dry well systems are currently experiencing localized flooding. The existing dry wells should be repaired to improve functionality of the system. In some locations a backup French drain system should be installed.

**Recommended Stormwater Infrastructure Improvements**

Table 9-4 shows recommended improvements to ensure adequate flow under current conditions and with implementation of the Specific Plan.

**TABLE 9-4** Recommended Improvements to Drainage System

Improvements	Location	Quantity	Unit	Existing Diameter (in)	Proposed Diameter (in)
Pipe Upsize	<b>Central Street</b> - Magnolia Street to Whitmore Avenue	550	LF	18	24
Pipe Upsize	<b>Whitmore Avenue</b> - Third Street to Second Street	600	LF	30	36
Pipe Upsize	<b>Whitmore Avenue</b> - Second Street to El Camino Lift	600	LF	30	48
Pipe Upsize	<b>Lawrence Street</b> - Fifth Street to Fourth Street	400	LF	18	24
Pipe Upsize	<b>Lawrence Street</b> - Fourth Street to El Camino Lift	250	LF	18	36
Pipe Upsize	<b>El Camino Avenue</b> <sup>1</sup> - Second Street to Detention Basin (Pine Street)	700	LF	18	36
Drainage Swale	<b>Fifth Street</b>	1,600	LF	—	—
Drainage Swale	<b>North Street</b> - Both Sides	1,900	LF	—	—
Drywell Repair	—	1	AL	—	—
French Drain	<b>Magnolia Street</b>	500	LF	—	24
Abandon Storm Drain	<b>Pine Street</b> - Across Highway 99	500	LF	18	—
Bore and Jack Storm Drain	<b>Pine Street</b> - Across Highway 99	500	LF	—	24
Manholes	—	20	EA	—	—
Drainage Inlets	—	13	EA	—	—

<sup>1</sup> Requires horizontal directional drilling under Highway 99.

The dry wells in Magnolia Street have a relatively large cumulative contributing stormwater shed, and thus this area should have a backup French drain system installed. However, streetscape improvements are planned for North, Third, Fourth, and Fifth Streets, and these areas are good candidates for the backup French drain system as well.

The City's Street Maintenance Department has indicated that the dry well drainage systems have been underperforming, in that the street tends to flood in areas served by these systems even in storm events that do not produce exceptionally large flow rates. One reason that the dry well systems are failing is that the systems were not well maintained until recently. The systems are fairly old, and some are starting to need replacement (e.g. corrugated metal pipe is rusting). In addition, the Street Maintenance Department fields many customer complaints regarding areas served by these systems. If these systems are to remain in place, the following actions should be taken to ensure their future adequacy:

- Analyze each rock well drain system and determine which are failing and need replacement.
- Replace damaged or rusting pipes.
- Perform outreach (e.g. mailers or newspaper notices) to educate customers on what dry wells are, how they work, and how to keep them functioning.
- Connect dry wells which handle flows from larger stormwater sheds to French drain systems.

Dry well systems are a “green” solution to stormwater and allow drainage to percolate into the soils and recharge groundwater aquifers. These dry well systems should be maintained to prevent inundation of the storm water pipe system and downstream basins. Table 9-4 describes improvements that could be made to the dry well system to improve performance.

The estimated cost for stormwater infrastructure improvements to support implementation of the Specific Plan is presented in Chapter 10, Implementation.

### **Wastewater**

Sewer demands in Downtown are serviced by the City's municipal sewer system and wastewater treatment plant. The sewer system serving Downtown discharges to the Pine Street Lift Station.

**Wastewater System Assessment**

The existing sewer system was examined and following assessments were made:

- The City of Ceres Waste Water Treatment Plant and the Pine Street Lift Station have adequate capacity to serve the Downtown.
- The pipe conveying effluent from the Pine Street Lift Station to the Waste Water Treatment Plant lacks capacity to serve the downtown area and other contributing sewer sheds and will need to be upgraded.
- Several wastewater pipes within the Downtown lack capacity and will need to be upgraded.

Table 9-5 shows existing wastewater flows in the Pine Street Lift Station sewer shed and the estimated flow increase from implementation of the Specific Plan.

**Recommended Wastewater System Improvements**

Table 9-6 shows recommended improvements to ensure adequate capacity and flow under current conditions and with implementation of the Specific Plan.

**TABLE 9-5** Sanitary Sewer Flow

	Water Shed	Acres	Average Dry Weather Flow (gpd)	Peaking Factor	Peak Dry Weather Flow (gpd)	Peak Wet Weather Flow (gpd)
Existing	Pine Street Lift Station	375	549,066	3	1,647,198	1,656,267
Proposed	Pine Street Lift Station	375	689,415	3	2,068,245	2,079,632

**TABLE 9-6** Recommended Improvements to Sewer System

Pipe Location	Improvement Limits	Length of Pipe (ft)	Existing Diameter (in)	New Diameter (in)
Fourth / Fifth Alley	North Street to Park Street	1,118	6	8
Ninth Street <sup>1</sup>	Magnolia Street to Roeding Road	2,095	8	10
Roeding Road	Ninth Street to Sixth Street	954	8	12
Sixth Street	Roeding Road to Park Street	392	8	12
Sixth Street	Magnolia Street to Roeding Road	1,693	8	10
Park Street	Sixth Street to Fourth/Fifth Street	608	8	15
Across Highway 99	Park Street to Pine Street Lift	336	12	18

<sup>1</sup> As an alternative to replacing the 9th Street main, a new line could be installed on Magnolia Street, from 9th Street to 6th Street and the 6th Street main could be upgraded from an 8-inch to 12-inch pipe. This would disrupt fewer services.

When designing improvements to the existing infrastructure, a field survey should be completed to ensure all contributing upstream sewer sheds have been accounted for in the downstream pipes and to verify sizes and slopes of existing pipes compared to current and needed capacity. In addition, existing sewer shed boundaries should be maintained to minimize the need for infrastructure upgrades downstream of the project area. Trenchless inspection of the system (i.e. TV inspection) should be utilized to determine the condition of the existing infrastructure with regard to cracks, tree root intrusion, corrosion, and other factors which could cause increased infiltration, and therefore decreased capacity, to the system.

Replacement of the sewer main from the Downtown to the sewer lift station will require horizontal directional drilling across Highway 99. The existing 12-inch pipe would most likely be abandoned in place. The existing pipe should be filled with controlled low strength materials or capped at each end. Another possibility would be to retain the existing pipe, but a secondary pipe would still be required.

During peak demands and peak wet weather demands, the Pine Street Lift station's outfall pipe from the lift station to the wastewater treatment plant is inadequate to convey the amount of effluent being pumped by the lift station's pumps. Other sewer sheds besides Downtown's combine with the outfall pipe and a larger scale study would be required to adequately size the remainder of the outfall pipe. Solving this problem will require a citywide effort, and costs are not included in this analysis. This issue is anticipated to be addressed with a wastewater master plan, scheduled to be prepared in 2010.

The estimated cost for wastewater infrastructure improvements to support implementation of the Specific Plan is presented in Chapter Ten, Implementation.

### **Dry Utilities and Undergrounding**

Electricity, natural gas, telephone, internet and cable television are available in the Specific Plan Area. Utility providers for electricity, natural gas, telephone, internet and cable television do not currently have plans for future improvements. These services follow demand and providers are prepared to provide adequate service to new development that occurs under the Specific Plan.

Utility undergrounding is recommended as appropriate. Undergrounding overhead utilities visible from Fourth Street should be a priority. Any utility undergrounding should be undertaken in coordination with new development and additional public improvements to ensure efficiency. While it is a long term goal of the Specific Plan for Downtown utility wires to be undergrounded where feasible and for developers to consider undergrounding in coordination with construction, decisions regarding details of utility undergrounding will be considered and evaluated on a case-by-case basis in consultation with City staff.

9 PUBLIC FACILITIES, SERVICES & INFRASTRUCTURE

## IMPLEMENTATION 10

This chapter describes the steps necessary to implement this Specific Plan. It provides both comprehensive strategies to guide Downtown’s redevelopment and technical guidance on how to implement the Specific Plan. The following sections discuss economic development, implementation and financing, and include a series of “next steps” the City should take to set the Specific Plan in motion.

## **A. Economic Development**

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Upon adoption of this document, the City must work strategically to attract and steer appropriate development and visitors toward Downtown. This section describes key economic development strategies, strategic phasing recommendations, project feasibility and potential marketing efforts.

### **Key Economic Development Strategies**

During implementation, the following key economic development strategies should be followed.

#### ***High-Quality Design***

High-quality design for new buildings, streetscapes and public improvements is essential for positioning Downtown for redevelopment. Appropriate preservation and adaptation of historic features or elements is important to reconnecting Downtown with its role in the City's history. At the same time, the judicious introduction of modern design should help establish Downtown as being at the leading edge of design for new uses and businesses. The juxtaposition of historic and modern elements can help create an environment attractive to the residents and businesses most drawn to downtown-style living. By implementing the Downtown Streetscape Standards and Design Guidelines in Appendix A, the City can ensure that high-quality design is strongly considered for all future improvements, whether public or private.

#### ***Downtown Entertainment Anchor***

In order to initiate redevelopment activities in Downtown, it is recommended that the City prioritize the recruitment of a major entertainment use, such as a cinema. A Downtown cinema should be able to house eight or more screens to be viable. Based on interviews with operators, there is current market interest in locating a new cinema in Ceres.

A cinema or some other entertainment anchor is desirable for Downtown because the foot traffic it generates will energize retail on adjacent streets and help attract additional restaurants and other entertainment uses. The City should take an active role in attracting a developer and cinema operator to Downtown, and should try to refocus any proposals for cineplexes elsewhere in Ceres towards Downtown.

***Land Assembly***

The small size of existing Downtown parcels makes land assembly difficult, time-consuming, and prone to failure. As a result, developers are often unwilling to enter into projects that require significant land assembly. Land assembly is most effectively done on a willing-seller basis. Implementation of the Specific Plan would not involve the use of eminent domain, except for those cases where specific property is needed for critical public infrastructure and other improvements that cannot be obtained from a willing seller. The City's Redevelopment Agency (RDA) should take an active role in facilitating land assembly in Downtown through a combination of joint ventures between property owners and developers, option agreements, and to the extent funding is available, on an outright purchase basis. Investments in land can be recouped from future sales to developers. The current economic downturn may provide a near-term opportunity to acquire targeted parcels at a lower cost than would have been possible during the recent real estate boom.

***Incentives for Development***

Parking requirements have the most significant impact on the density, size and types of development that can occur on a given site. For mixed use development, small parcels can create inefficient parking layout and limit project size. The cost of providing new parking, particularly in expensive parking structure projects, is a major factor affecting project feasibility for developers. Mixed use development can generate significant efficiencies when uses have different peak periods (e.g., offices require daytime parking, while apartments require nighttime parking).

The Specific Plan significantly reduces development parking costs by creating new angled parking on Downtown streets that can help meet project parking requirements, requiring lower parking ratios, and offering incentives for the provision of shared parking in alleys. The creation of mid-block surface parking behind mixed use projects in alleys will help meet a project's parking requirements and increase parking availability for Downtown shoppers, residents and businesses. Incentives could also be used to enhance pedestrian circulation from mid-block areas to Downtown streets. There is also the potential to create one or more moderate size off-street public surface parking areas in order to assist developers in meeting some of their parking requirements. This could increase the feasibility of projects by allowing developers to construct a larger building on a given site. The cost of such a parking area could be recovered from sale of credits to developers that would be applied to their project parking requirements.

***Strategic Assistance for Catalyst Projects***

The pioneering nature of mixed use and other development types envisioned for Downtown may limit developer interest. Until examples of these projects are successfully developed, the Downtown area will be considered risky and unproven to developers. The RDA can address this by identifying and assisting selected “catalyst” projects, including providing targeted financial assistance to reduce developer risk. The successful implementation of catalyst projects should stimulate greater interest from other developers in similar new projects, which should not require further financial assistance from the RDA.

Potential projects that may be appropriate for assistance from the City include the cinema (or other Downtown anchor) and an initial mixed use project on Fourth Street. A mixed use catalyst project would be appropriate at several locations on Fourth Street, including the southwest corner of the Whitmore Avenue/Fourth Street intersection, the northwest corner of the North Street/Fourth Street intersection. Furthermore, a pure commercial project would be appropriate at the southern end of Fourth Street, where the Valero gas station is currently located.

***Public Private Partnerships***

The City should strongly promote the benefit of entering into public-private partnerships to achieve projects. The RDA may enter into Disposition and Development Agreements (DDAs) for publicly owned properties, leading to their sale or long-term ground lease for new development through public/private partnerships. The RDA may also enter into Owner Participation Agreements (OPAs) with private property owners to assist them in redevelopment of their properties, also setting the stage for public/private partnerships. As one of the terms and conditions of such partnerships, the City could seek to have developers perform streetscape improvements, make parking facilities on their properties available to off-site users as part of the creation of the mid-block parking system, or undertake other improvements.

**Development Phasing**

The Specific Plan envisions three five-year phases of development with a total plan implementation timeframe of up to 15 years. This section provides general direction for the phasing of development and public improvements. The phasing strategy is conceptual and should remain flexible in order to respond to future market forces.

**Phase 1**

The primary focus of Phase One is the development of an anchor cineplex entertainment use at the south end of Fourth Street, along with nearby retail and dining uses. Mixed income housing should be provided in an initial set of mixed use projects with ground floor retail, along with other townhouse projects. Phase One should also include the development of new buildings at designated Downtown gateways to catalyze revitalization. Total development in this phase would include approximately 125 residential dwelling units, including both rental and for-sale housing, and approximately 85,000 square feet of new retail and office space, including the cineplex.

**Phase 2**

Phase Two includes the development of additional mixed use retail, office, and residential development farther north along Fourth Street and elsewhere in Downtown. Residential development includes infill townhouses and multi-family residential projects. Public improvements include the rehabilitation of Downtown parks and the creation of paseos, as well as the redevelopment of the Civic Center area for City offices. Total development would be approximately 250 residential dwelling units and approximately 78,000 square feet of new retail and office space.

**Phase 3**

Phase Three includes the remaining buildout of the Specific Plan. Development in this phase includes remaining residential, office, and retail mixed use projects along Whitmore Avenue and at additional infill locations in Downtown. Residential development includes infill townhouses and multi-family residential projects. Class A office space along Central Avenue with visibility from State Route 99 is also included in Phase Three, along with the completion of redevelopment of the Civic Center area. Total development includes approximately 151 residential dwelling units, and 120,000 square feet of new retail and office space.

The anticipated phasing reflects a modest start to Downtown revitalization, recognizing the current economic challenges that are likely to persist through 2011 and beyond, as well as the challenges of attracting new development and investment activity to an unproven market. The development program is summarized in Table 10-1.

**TABLE 10-1** Ceres Downtown Specific Plan Private Development Phasing

RESIDENTIAL - Units (a)	Phase 1 Years 1-5	Phase 2 Years 6-10	Phase 3 Years 11-15	Total
Single-Family/Multi-Family	125	250	151	526
<b>NON-RESIDENTIAL - Sq. Ft. (b)</b>				
Commercial/Retail	30,000	44,000	60,000	134,000
Cinema	40,000	0	0	40,000
Office	15,000	34,000	60,000	109,000
<b>Total Non-Residential</b>	<b>85,000</b>	<b>78,000</b>	<b>120,000</b>	<b>283,000</b>

Private Development defined as property-tax paying projects, does not include new civic, school district, church, or other tax-exempt institutional uses.

(a) The total number of units shown represents the gross number of new residential units projected. It is assumed that approximately 31 existing units will be replaced by these new units.

(b) The total square footage shown represents the gross number of new retail and commercial square footage projected. It is assumed that 68,000 square feet of existing commercial/retail and 22,000 square feet of existing office will be replaced.

Sources: DC&E; BAE, 2009.

### Downtown Development Feasibility

This section describes Downtown’s market positioning for new development on a programmatic level, as well as for retail, office and residential uses.

#### Specific Plan Feasibility

Based on the relationship between 2008 market rents and sale prices for new development in Ceres, which is already lower than the market peak in the previous economic cycle, and development costs; new development as described in the Specific Plan appears to be financially feasible without substantial public assistance. While the current recession has lowered market rates and sale prices, recovery to 2008 levels is anticipated by the end of the first phase of Specific Plan implementation. The one exception would be podium mixed use development; rental rates and sale prices would have to rise considerably above current levels for this more expensive type of construction to be feasible.

Downtown is mostly suited to smaller-scale development by virtue of its smaller blocks and parcel sizes. The City should capitalize on these urban design characteristics by targeting development that will not occur elsewhere in the area, rather than trying to duplicate types of larger-scale development that would be inappropriate for Downtown. Although some land assembly may be required to make

projects feasible, very large sites are not necessary for most development types envisioned for Downtown. Smaller-scale development in Downtown can be complemented with uses that benefit from freeway visibility and access on selected sites with freeway proximity at Fourth Street and Whitmore Avenue. Creating a unique mix of development types can help establish a distinctive identity for Downtown and establish it as a destination.

### ***Retail Feasibility***

Retail products should be targeted to small (1,000-5,000 square feet) and medium-sized (approximately 10,000 square feet) retailers. Small and mid-sized retail spaces are well suited to a mix of high quality local retailers as well as regional retailers interested in a downtown setting. The City should target independent retailers who have been successful in Downtown Modesto and other Central Valley downtowns, and would be interested in adding a location in an emerging downtown retail setting.

Larger retailers (e.g. 20,000 square feet or larger) would be difficult to accommodate within Downtown's existing street grid without providing either a large amount of nearby surface parking, which would negatively impact Downtown's attractiveness, or a parking structure that would be prohibitively expensive and undesired by such a retailer. These types of retailers are typically larger, regional and national chains that would be difficult to attract to Downtown until it is a well-established retail location.

### ***Office Feasibility***

Office spaces should consist of a variety of smaller spaces oriented towards professional services, local creative services and other types of local businesses. Another possibility is the development of office condominiums of 5,000 square feet or less, which are targeted at firms that want to own their own space. Office condominiums are spaces within office buildings that are sold space-by-space to individuals.

### ***Residential Feasibility***

For-sale and rental multi-family housing (with 15 percent inclusionary units per Community Redevelopment Law requirements), townhouses, and live/work units are recommended for Downtown. These units can be targeted at a range of households, including young singles and families seeking more affordable units, "empty nesters" looking to downsize, and area artists, craftspeople, and others looking to combine work and living spaces in a live/work unit.

## **Downtown Marketing**

### ***Visibility, Identity and Critical Mass***

Downtown currently suffers from a lack of visibility and identity due to its obsolete structures and limited and weak tenant mix. The City should encourage additional promotional and community events, particularly as new Fourth Street streetscape improvements are completed. Fourth Street should be designed and programmed as Ceres' "living room," a setting for large-scale public ceremonies and events, as well as live entertainment, arts events, festivals and other similar activities.

The Specific Plan Area should be referred to as "Downtown Ceres" in publicity materials, event descriptions, marketing activities and other media to promote Downtown's branding. It is recommended that these measures be taken in the early stages of implementation.

As the City assembles parcels, provides assistance to catalyst projects, and makes phasing decisions, it should also seek to cluster new development geographically. Dispersing development over a larger area can reduce its visibility and impact. Promoting nodes of new development in each phase helps those projects become more successful and encourages developers and investors to seek out the next nodes for redevelopment.

The key node for Phase One is around the cineplex site and adjacent buildings. Phase Two should focus on the blocks between the Whitmore Mansion and the Civic Center. Phase Three should focus on Central Avenue, which is identified for office development.

### ***Developer Recruitment***

The Specific Plan envisions a range of small to medium-sized development projects with values of approximately \$10 million to \$30 million. This is too small to attract larger developers, but in many cases would also be too large for smaller developers. To overcome the challenges of creating a market for development in Downtown, it will require developers with previous experience successfully completing similar projects.

There are a number of small to mid-size developers that specialize in creative revitalization of smaller downtown areas in California. The City should work to compile a mailing list to target developers it would like to attract, and commence a marketing program to introduce the Specific Plan, offer familiarization tours, assist with identification of prospective sites and available properties, and provide continuing updates on Downtown's progress, activities and evolving opportunities.

**Retailer Recruitment**

The Specific Plan envisions a dynamic mix of existing Downtown retailers, new high-quality local and regional retailers, and an appropriate number of California and national retailers. To support this, retail recruitment should begin in Phase One.

Local retailers with experience operating one or more locations and that have the potential to expand their businesses, are prime prospects for the early rounds of leasing in Downtown. This includes existing retailers in Ceres and nearby cities, such as Modesto and Turlock, as well as retailers elsewhere in the Central Valley. These retailers often have the experience and knowledge needed to attract customers, and understand local residents and their interests. At the same time, these are not the types of retailers who may be identified or respond to typical leasing activities for new centers, nor are they the types of tenants that leasing brokers necessarily target. As a result, the City must take a proactive approach to recruitment.

Other small retail districts have successfully engaged retail recruiters to develop and implement retailer recruitment strategies. Strategies identify and engage strong local retailers and determine the retail business types that are most appropriate, and then match retailers with available spaces. Retention by the City of a retailer recruitment specialist could complement the leasing activities that would be undertaken by individual developers.

Priorities for retailer recruitment include entertainment, arts-related, restaurant and unique types of retail uses that can complement the cinema anchor. Later phases could focus on specialty grocery and services that serve as amenities for new Downtown residents and businesses. It may also be appropriate to attract national retailers in later phases of implementation once Downtown redevelopment has developed significant momentum and becomes more attractive to these retailers.

**Ceres Downtown Revitalization Area Board**

Expand the role of the Ceres Downtown Revitalization Area Board to undertake an enlarged marketing, promotion and special events program. This will help increase resident and visitor awareness of Downtown, as well as promote future business in Downtown.

## **B. Specific Plan Implementation and Financing**

This section provides a detailed description of the technical steps that must be taken by the City to implement this Specific Plan. This section also provides an overview of potential funding sources and a conceptual financing strategy.

### **Specific Plan Implementation**

This section describes the technical steps necessary to adopt the Specific Plan and certify the associated Environmental Impact Report (EIR).

#### ***General Plan Amendments***

Adoption of this Specific Plan requires an amendment to the City's General Plan to make the following changes:

- Include new General Plan Land Use Designations for Downtown Mixed Use, Downtown Office and Downtown Residential.
- Revise the General Plan Land Use Diagram (Figure 1-2) and the "Summary of Land Use Designations and Standards" (Table 1-1) to reflect the new designations from the Specific Plan.
- Revise the Bicycle Routes Diagram (Figure 2-2) to show Lawrence Street as a Class III bike route.

#### ***Specific Plan Overlay Zone***

Adoption of the Specific Plan will require the following amendments to the City's Zoning Code:

- Add a new zoning classification called "Downtown Specific Plan Overlay Zone." This amendment must include clarifying text stating that the Downtown Specific Plan will govern development standards in most cases and also provide direction for those provisions that are not accounted for in the Specific Plan.
- Revise the Zoning Map to replace existing zoning in the Specific Plan Area with the Specific Plan Overlay Zone.

#### ***Review of Future Projects***

The concepts recommended in this Specific Plan have been approved by the City and studied in the Initial Study prepared for the Plan. Future development applications that adhere to the concepts recommended in this Plan may only be subject to minor additional environmental review.

No subdivision map, use permit, design review application or other entitlements will be issued for development projects in the Plan Area until a finding has been made that the proposed project is consistent with the Specific Plan.

## **California Environmental Quality Act (CEQA) Requirements**

### ***Certification of CEQA Document***

The City of Ceres certified the EIR for the Downtown Specific Plan on XXXX, 2010. The City's action to certify the EIR did not constitute approval of the Specific Plan. Rather, it indicates that the EIR has been completed in compliance with CEQA, and that the EIR was presented to and reviewed by the City's decision-makers and the public prior to Specific Plan approval.

### ***Mitigation Monitoring Program and CEQA Findings***

Public Resources Code Section 21081.6 requires that a "reporting or monitoring program be designed to ensure compliance during project implementation." The adopted program shall apply to changes made to the project or conditions of project approval in order to mitigate or avoid significant effects on the environment. The monitoring program provides a brief summary of the required mitigation for impacts attributable to the project, identifies the party responsible for monitoring the project's compliance with the mitigation measure, and identifies at what point or phase of the project the mitigation measure is to be completed. The City has prepared a mitigation monitoring program in conjunction with the preparation of the EIR for the Specific Plan. The EIR identified several environmental impacts that could be reduced to less than significant with the implementation of the mitigation measures.

## **Funding Sources and Financing Strategy**

While private development is the primary mechanism for revitalization of Downtown, it cannot occur without assembling parcels to create large enough sites for new projects, as well as public improvements, such as streetscape enhancements and the provision of parks and paseos, parking and infrastructure, including storm drainage, sewer and water systems. These public actions and improvements are needed to help leverage private investment and reduce the perceived risk for developers and their investors. Public investment and assistance for catalyst projects should be prioritized in early phases of implementation.

There are a variety of financing tools available to cover the cost of City actions and public improvements. These include developer-financed improvements and land dedications pursuant to development agreements, public-private development partnerships that reimburse the City's cost of assembling developable sites, and other types of public-private partnerships (as described in Section A) that result in improvement projects, such as parking lots that increase the feasibility of development on nearby properties.

There are also a variety of public financing sources that utilize a combination of debt (e.g., bonds) and grant sources that would be considered for the financing plan. These sources would utilize new fiscal revenues generated by redevelopment in Downtown, such as property taxes, sales taxes and transit occupancy taxes, new fees or funds from federal, state or regional grant programs for eligible projects.

There are, in addition to the sources shown below, other bonding sources that could be used. However, since these would rely upon commitments from existing General Fund sources, they are not included here because of the impact upon current activities supported by the General Fund.

### Public Funding

- **Tax Increment Financing (TIF).** The Specific Plan Area boundaries coincide with a Redevelopment Project Area. The City's RDA has recently obtained significant funding for the redevelopment Capital Improvement Program (CIP). However, to avoid competition with planned projects, this implementation strategy evaluates the tax increment that would be generated from new Downtown development.
- **Affordable Housing Set-Aside Funds.** Community redevelopment law requires that 20 percent of all new tax increment funds be set aside for affordable housing production. These funds can be used for either rental or for-sale housing for low and moderate income households. The RDA is currently using these funds to support affordable developments, but expects to have approximately \$1 million in available funds for other projects. The affordable housing set-aside fund will be augmented by the 20 percent share of tax increment from new Downtown development.

Existing plans for an affordable multi-family project Downtown, an area which has not seen recent new housing investment and in a market that has not developed significant market-rate multi-family ownership housing, could help generate interest by market-rate developers.

Using the Specific Plan’s projected amount of new development over the next 15 years, and assumptions from previous pro forma analysis of potential projects, projections were prepared to estimate the increase in assessed value, available tax increment to support redevelopment, and set-aside funds for affordable housing development. These projections are summarized in Table 10-2.

**TABLE 10-2** Projected Downtown Ceres Project Area Tax Increment Distribution by Specific Plan Phase

Figures are for the end of each phase, except as noted. Increment is calculated for new Downtown development, reassessment upon sale of existing Downtown buildings, and Proposition 13 annual 2% increase for all properties.									
Plan Phase / Years	Total Assessed Value	Annual Gross Tax Increment	Annual Housing Fund 20%	Annual Statutory Pass-Throughs				Annual County Fee 1.2%	Annual Net Increment at End of Phase
				Tier 1 Payments 25%	Tier 2 Payments 21%	Tier 3 Payments 14%	Total Annual Statutory Pass-Through		
1 Year 5	\$54,200,000	\$569,269	\$113,854	\$113,854	\$95,637	\$0	\$209,491	\$6,547	\$239,378
2 Year 10	\$153,477,823	\$1,611,996	\$322,399	\$322,399	\$270,815	\$0	\$593,215	\$18,538	\$677,844
3 Year 15	\$244,059,332	\$2,563,384	\$512,677	\$512,677	\$430,649	\$287,099	\$1,230,425	\$29,479	\$790,804

Note: Per AB 1290, Tier 1, Tier 2 and Tier 3 pass-throughs are applied after the Housing Fund pass-through is deducted from the Gross Tax Increment.

Sources: City of Ceres Redevelopment Agency; Bay Area Economics, 2009.

New development by the end of Specific Plan implementation would increase the assessed value of Downtown property by \$244 million in current dollars. The annual increment available for redevelopment activities would be just slightly under \$800,000, with approximately \$500,000 per year generated for the affordable housing set-aside fund. To the extent possible, available tax increment from the Specific Plan Area should go directly to improvements in the Specific Plan Area, including infrastructure financing. This is typically a key source of income for projects in the redevelopment area to have the City commit tax increment financing (TIF) to repayment of infrastructure. It should be noted that these amounts are considerably lower than the gross tax increment because of the age of the Project Area, which was established in 1991 and results in a larger share of new tax increment going to other taxing jurisdictions as pass-throughs, pursuant to the provisions of California Community Redevelopment Law. However, the larger pass-through amount means that the City’s General Fund portion of the pass-through amount is higher.

- **Local Share of New Sales Tax Receipts.** A new cinema and other new Downtown retail would generate substantial new amounts of sales tax receipts for the City’s General Fund, based on the one-percent share received by local governments. These proceeds could be used to help finance some of the public improvements for Downtown revitalization that help attract those businesses.
- **Utility Connection Fees.** The City has an existing program of utility connection fees for sanitary sewer and domestic water systems that helps cover the cost of improvements to these systems. Except for a new well or a major water transmission main on Central Avenue, the fees that would be generated by new Downtown development would not be used to help cover some of the costs for needed improvements to these systems under the City’s current fee structure. As a result, it is anticipated that and recommended that the City’s utility fee structure be amended to ensure additional funds are targeted toward Downtown in Phases 2 and 3. Connection fees assumed to be collected for new development are estimated and included in the Table 10-3, which is presented later in this chapter.
- **Development Impact Fees.** Impact fees are fees levied upon new development to mitigate the effects of that development. Establishment of an impact fee requires documentation through a study that meets the requirements of AB 1600 to establish a clear nexus between the fee to be collected and the improvements that will mitigate the impact of development.

Certain public improvements in the Specific Plan are well suited to use of impact fees, such as traffic improvements. It is estimated that improvements at the El Camino Avenue/North Street/Highway State Route 99 Ramps intersection described in Chapter Eight, Circulation, will cost \$1.2 million. Although the current traffic impact fee does not include this improvement, these new transportation improvements could be targeted to mitigate impacts created by new Downtown development and could be funded with the collection of a traffic impact fee, which would be subject to completion of a nexus study. The City also currently collects an impact fee for land acquisition and construction of park capital improvements. Since the nexus study for that fee did not anticipate new Downtown development at the scale envisioned in the Specific Plan, it may be possible to use the capital improvements portion of the park impact fee collected from new Downtown development to finance rehabilitation and improvement of Downtown parks. This will require further evaluation.

Other Specific Plan improvements may not be as well-suited to impact fees because the work required includes dealing with existing capacity and condition issues, as well as expansion of facilities to support new development. This applies

to infrastructure work for storm drainage, sewer and water system improvements, which would be primarily funded through new connection fees paid by developers. If gaps in infrastructure financing are experienced, an impact fee for infrastructure may be appropriate, but would require a nexus study and should be at a rate that will not significantly discourage development.

- **City Capital Improvement Program (CIP).** The City’s ongoing CIP program could be used to close financing gaps that cannot be addressed through use of other sources. This is a “last resort” source because the existing CIP program is underfunded. However, increases in the City’s tax base from new Downtown development after needed improvements have been made could ultimately generate money for the City’s General Fund that could be used to finance CIP work elsewhere in the City.

### Additional Funding Opportunities

The following funding opportunities may also be appropriate to assist in financing improvements in Downtown:

- **Transportation Funding.** Federal funds for transportation and congestion-related improvements that benefit transit and encourage alternative modes of travel. Federal sources often have a 20 percent local matching funds requirement.
- **Economic Development and Neighborhood Grants.** Economic development and neighborhood improvement grants for public facilities, including those addressing blight and benefiting lower- or moderate-income persons.
- **State Initiative Funds.** Proposition 1C provides funds for transit-oriented development, housing and parks. Proposition 1B provides funds for congestion-related improvements.
- **Environmental Enhancement Funds.** Environmental enhancement programs, such as those that address air quality or highway landscape, may provide opportunities for the future transit corridor.
- **New Markets Tax Credit.** The census tract for Downtown Ceres is eligible for use of the New Markets Tax Credit (NMTC). This federal program provides a structured financing technique that can enhance the returns of investors and/or lenders to stimulate new development and businesses in disadvantaged areas. However, unlike other federal tax credits such as the historic preservation tax credit or Low Income Housing Tax Credits, the NMTC program does not effectively provide no-cost equity to development projects. Thus, NMTC only works for projects that are able to generate somewhat of a return on investment, and can use the tax credit to augment that rate of return to a competitive level. NMTC may

be an appropriate tool for both prospective developers and new business owners in Downtown to obtain by working with eligible community development entities that distribute the credits.

Certain General Fund sources were not considered for this implementation strategy. Existing demands on the General Fund do not permit expenditures to support Downtown development except as a last resort to close financing gaps. Furthermore, new property tax increment that flows from Downtown development to the City’s General Fund needs to be reserved to cover increased public service costs for new residents and businesses. Excess impact fees collected from development would also not be appropriate for use in Downtown. Impact fees would generate in excess of \$7 million, however, aside from a portion of capital improvement park impact fees, these funds will be used to offset development impacts elsewhere in the City created by Downtown development.

**Conceptual Financing Strategy**

The Specific Plan identifies a series of street, streetscape, transportation, park, paseo and infrastructure improvements that are needed for implementation. The estimated total cost of these improvements is approximately \$22.5 million, including hard and soft constructions costs, as well as a 25 percent cost contingency. These costs also include land acquisition for public improvements, such as parking lots and paseos, however it does not include land acquisition for land assembly to create development sites, as it is assumed most of these costs would be recovered from sales to developers.

Analysis was conducted to determine how these costs could be phased over time, to better match the timing and types of expected development, as well as available funding sources. It is believed that the first phase of Specific Plan development would require an expenditure of approximately \$7 million in public improvements, the second phase would require approximately \$11 million, and the third phase would require approximately \$4 million.

Table 10-3 shows projections of funding sources by phase, as well as needed expenditures by phase.

The projected TIF bond amount of approximately \$7 million is a conservative estimate based on the amount of increment projected to be available for debt service by the end of the second phase. An allowance of \$3 million has been identified for potential federal and state grants, however the actual amount will be determined by future grant offerings, City applications and funder actions.

**TABLE 10-3** Downtown Ceres Specific Plan - Potential Sources & Uses of Funds (*Estimates in Constant 2009 Dollars*)

SOURCES OF FUNDS	Phase 1 Years 1-5	Phase 2 Years 6-10	Phase 3 Years 11-15	Total
New Tax Increment Bond (a)	\$0	\$7,000,000	\$0	\$7,000,000
Local Share of New Retail Sales Tax	0	0	1,400,000	1,400,000
Utility Connection Fees (b)	500,000	1,250,000	800,000	2,550,000
Water & Sewer Enterprise Funds (c)	625,000	625,000	0	1,250,000
Park Development Impact Fees (d)	200,000	300,000	300,000	800,000
New Transportation Dev. Impact Fee	300,000	400,000	500,000	1,200,000
City Redevelopment Bond Funds (e)	5,300,000	0	0	5,300,000
Grants (f)	1,000,000	1,000,000	1,000,000	3,000,000
<b>Total Sources</b>	<b>\$7,925,000</b>	<b>\$10,575,000</b>	<b>\$4,000,000</b>	<b>\$22,500,000</b>
<b>USES OF FUNDS (g)</b>				
Transportation Improvements (h)	\$0	\$350,000	\$850,000	\$1,200,000
Street & Streetscape Improvements (i)	1,750,000	1,350,000	1,800,000	4,900,000
Park Rehabilitation	0	5,100,000	700,000	5,800,000
New Paseos	0	2,100,000	0	2,100,000
New Public Parking Lot	0	600,000	0	600,000
Storm Drainage Improvements	3,800,000	300,000	500,000	4,600,000
Sewer Improvements	1,200,000	1,000,000	300,000	2,500,000
Water System Improvements (j) (k)	300,000	400,000	100,000	800,000
<b>Total Uses</b>	<b>\$7,050,000</b>	<b>\$11,200,000</b>	<b>\$4,250,000</b>	<b>\$22,500,000</b>
<b>OTHER FUNDING SOURCES</b>				
Affordable Housing Set-Aside (l)	\$1,400,000	\$1,300,000	\$2,700,000	\$5,400,000

See narrative for additional assumptions and limiting conditions:

- a Potential tax-exempt bond financed from new tax increment from new Downtown development. Approximate figure for 20-year bond, 5% rate, with reserves and underwriting costs.
- b Based on existing connection fees for sewer and water system. Use for Phases 2, 3 will require Council change to approved utility improvement program.
- c Funding from City's existing water and sewer enterprise funds to cover costs to correct deferred maintenance.
- d Allowance of up to 50% from existing park impact fee for capital improvements portion, generated from new Downtown housing units not in original nexus calculation.
- e Allocation of City's approved Redevelopment Bond Funds for Phase 1 work.
- f Allowance for combination of federal and state grants. See text for explanation.
- g Cost figures from DC&E, Jacobs Engineering, includes hard and soft costs, 25% contingency. Also includes land acquisition for Paseos, new City lot. Expenses for site assembly assumed to be recovered from sale to developers in the same phase, with no write-down, resulting in minimal net expense.
- h Transportation improvements are described in Chapter Eight, Circulation.
- i Utility undergrounding costs are included for North Street, Fifth Street and Whitmore Avenue.
- j Water system costs reflect Option 2, as discussed in Chapter Nine, Public Facilities, Services and Infrastructure.
- k Includes Phase 3 cost reduction from water tank construction no longer required.
- l Statutory set-aside of 20 percent of available increment from new development; Phase 1 includes projected available balance. Funds can be used to meet statutory requirement for 15 percent affordable housing production in project area.

Sources: City of Ceres; DC&E; Jacobs; BAE, 2009.

There is an overall shortfall of approximately \$5.3 million between the sources of funding and identified public expenditures. To bring the sources and uses into balance, it is assumed that the \$5.3 million could be funded over the three phases from the City's Redevelopment Bond Funds if it is amended or prepared to accommodate proposed improvements.

The financing strategy shown in Table 10-3 should be considered preliminary, and subject to revision and refinement based upon further evaluation of funding source availability and suitability.

## C. Next Steps

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This section presents a series of next steps that could be undertaken in the near term to begin the Specific Plan implementation process. All of these actions should be initiated in Phase One of Specific Plan implementation.

### **Develop Specific Branding and Marketing Strategy**

The City should immediately begin working on a strategy to brand and market Downtown. The strategy should be comprehensive and cover branding and marketing efforts to potential visitors, developers and retailers. This effort will be critical to the successful revitalization of Downtown. In the early stages of this process, a branding and marketing campaign will show the City's commitment and desire to initiate positive change in Downtown. As part of this effort, the City of Ceres should create a comprehensive Economic Development and Marketing Plan that establishes a clear strategy for marketing Downtown to developers and the general public.

### **Adopt and Implement Standards and Guidelines**

Ensure high-quality design, both for projects by new developers and businesses, as well as for public improvements. Seek to combine the preservation of historic elements with more modern design. This can be achieved by following the development standards in this Specific Plan and by requiring new development and public improvements to follow design guidance in the Downtown Streetscape Standards and Design Guidelines in Appendix A.

### **Initiate Design Processes for Public Improvements**

Although this Specific Plan proposes several conceptual public improvements, additional design must be carried out for key projects, such as streetscape improvements. Once funding is secured, commence more detailed design processes for public improve-

ments as appropriate, including more detailed design proposals, cost refinements, and evaluation of opportunities for phasing and identification other funding sources.

### **Design a Wayfinding and Signage System**

Design should commence to develop a comprehensive Downtown wayfinding plan and signage system. The design work should result in a plan that details sign design and locations so as to improve visibility and access to Downtown.

### **Initiate Efforts to Revise Infrastructure Fee Structure**

Revisions to the current infrastructure fee structure need to be considered in order to support Downtown infrastructure improvements to support new Downtown development envisioned in the Specific Plan. City departments should work together as development takes place in the Specific plan Area to ensure that both citywide and Downtown-specific infrastructure needs are adequately addressed under any potential revision.

### **Initiate Recruitment Efforts for Downtown Anchor**

Create a public-private partnership to establish a cinema as the anchor entertainment use for Downtown, locating it at the South end of Fourth Street, and preclude its being located at another new center outside Downtown.

The City needs to be proactive in attracting this use to Downtown, including initiating discussions with interested cinema operators and working with the numerous affected property owners on land assembly for the project. Other near term steps related to a Downtown anchor include conducting project-specific feasibility analyses and schematic design to identify and address development issues, and exploring the potential public role in a public-private partnership for this anchor project.

### **Initiate Land Assembly**

The City should be proactive in assembling potential development sites in Downtown. Assembly of land for Downtown projects will be a difficult challenge, so the City should undertake thorough review when land acquisition and consolidation opportunities arise.

### **Explore Potential Incentives for Development**

Provide incentives to developers to facilitate the creation of mid-block parking in alleys and on-street parking to meet developer parking requirements, thereby enhancing project feasibility. Density bonuses may also be an appropriate incentive to offer

developers to increase project feasibility, and could also result in new amenities that would be accessible by the public. The Specific Plan already has policies in place for providing incentives for development that provides shared parking. The City should remain open to and consider what additional incentive programs might be appropriate for Downtown.

### **New Downtown Development Fees**

A Downtown Traffic Impact Fee is already recommended as part of this Specific Plan to fund intersection improvements to support traffic generated by Downtown growth. The City should consider if other impact fees may be appropriate. For any impact fee, the City must perform a nexus study that finds a relationship between new development and identified improvements. It is recommended that total impact fees for new development do not exceed five percent of total development costs.

### **Ongoing Evaluation**

Continue to evaluate the policy, legal, and other issues associated with the reallocation of existing park impact fees; the creation of a new transportation improvements impact fee for Downtown; and other funding sources.

# **APPENDIX A: DOWNTOWN STREETScape STANDARDS & DESIGN GUIDELINES** **A**

These Streetscape Standards and Design Guidelines are an important companion to the Downtown Specific Plan. The goal is to ensure that new streetscape or private development projects support the quality and character of the vision spelled out in the Specific Plan. The standards and guidelines provide a framework for property owners, architects, and City staff to follow when planning development projects in Downtown. They will also be used by City staff and the Planning Commission to review new development and renovation projects in the Specific Plan Area.

## A. Organization of Streetscape Standards and Design Guidelines

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This document consists of two parts: Downtown Streetscape Standards for streetscape projects that are within the public right-of-way, generally funded and constructed by the City; and Design Guidelines for individual building and renovation projects on parcels, generally funded and constructed by landowners or private developers.

Downtown Streetscape Standards in Section C provide standards for how sidewalks, on-street parking, travel lanes, street trees, lighting, street furniture, and bicycle facilities and parking should be provided on all Downtown streets. The Streetscape Standards should be referenced and applied in future streetscape planning. They are intended to be adjusted to accommodate specific conditions during a detailed design development process. Future streetscape designs should comply with these standards wherever possible to ensure a safe, attractive and pedestrian-friendly environment, and an environment that is unique to Ceres.

Design Guidelines in Section D are intended to provide firm yet flexible guidance for future development projects within Downtown. They address site planning and building design by District, and landscape design, lighting and signage on a Downtown-wide basis. The Guidelines should be used as a guide for the review of all of these elements in Downtown development projects.

## **B. Guiding Principles**

The following principles provide the context and background for the Streetscape Standards and Design Guidelines that follow.

### **Sustainability**

Private development and public improvements in Downtown shall strive to minimize impact on the natural and built environment. Encouraging visitors and residents to walk or bicycle throughout Downtown will reduce the impact of driving in the form of traffic, air quality, and climate change. Buildings should be constructed to save energy and use green materials. Streets and projects should help manage stormwater, and be planted with drought tolerant landscape. Lighting should be energy efficient, and exterior lighting should be designed and specified with “dark sky” technology. “Dark sky” is a term used to describe a strategy to reduce the negative impact of too much nighttime lighting, which is increasingly limiting our view of the night sky.



### **Respect for Context**

Downtown Ceres contains a number of significant historic buildings and open spaces, including the Whitmore House and Whitmore Park. New public improvements and private development should respect the scale and character of their neighbors by requiring appropriate setbacks and height limits and by limiting the general scale of development near them. Structures older than 50 years in age should be assessed to determine their architectural significance prior to any new development. Additionally, new development should consider adjacent residential uses to ensure that its physical characteristics and siting does not interfere with existing properties. The impact of lighting and other elements should be considered and mitigated in the design of future projects, particularly when projects are adjacent to residential uses.



### **Quality of Design, Materials and Construction**

Streetscapes, buildings and open spaces should be constructed with the quality befitting the downtown of a significant city. Buildings should be well proportioned with generous windows and entries. They should be constructed from materials that are attractive and durable. Additional elements, including signage and lighting fixtures on buildings, should be designed in coordination with building materials and at an appropriate scale.

### **Building Orientation**

The main faces of buildings should be oriented toward public space, which in Downtown Ceres means the street front. All building entries should be entered from the street or from paseos, rather than parking lots. Commercial buildings should be built close to the property line at the sidewalk. Buildings on corners should acknowledge the corner with a tower, special roof, entry or other special feature.



### **Building Scale and Massing**

The massing of larger buildings should be broken up into smaller components that relate better to a human scale, and to the traditional parcel size in Downtown. For example, a ninety-foot long building should be vertically separated into three thirty-foot parts. This creates more interest and it rewards attention from visitors and pedestrians.



### **Pedestrian-Friendly Streets and Spaces**

Streetscape design, site planning, building design and landscaping of private and public projects should all consider pedestrian needs. For Downtown Ceres this includes shade from trellises, awnings and trees; places to sit and chat; generous, well-detailed and maintained sidewalks and paseos; and lots of interesting storefronts, artwork and landscaping to walk past. Car parking should be broken up into smaller pools, and street traffic should be calmed. If pedestrians feel welcome they will attract other pedestrians, leading to a vibrant and friendly Downtown environment.

### **Crime Prevention Through Environmental Design (CPTED)**

Streetscapes and private development should be designed to ensure safety and crime prevention by applying the principles of Crime Prevention Through Environmental Design (CPTED). CPTED is a crime prevention philosophy based on the theory that the proper design and effective use of the built environment can lead to a reduction in the public perception and incidence of crime. CPTED principles include natural surveillance or “eyes on the street” as well as territorial reinforcement, which means a clear sense of ownership of space, both public and private.

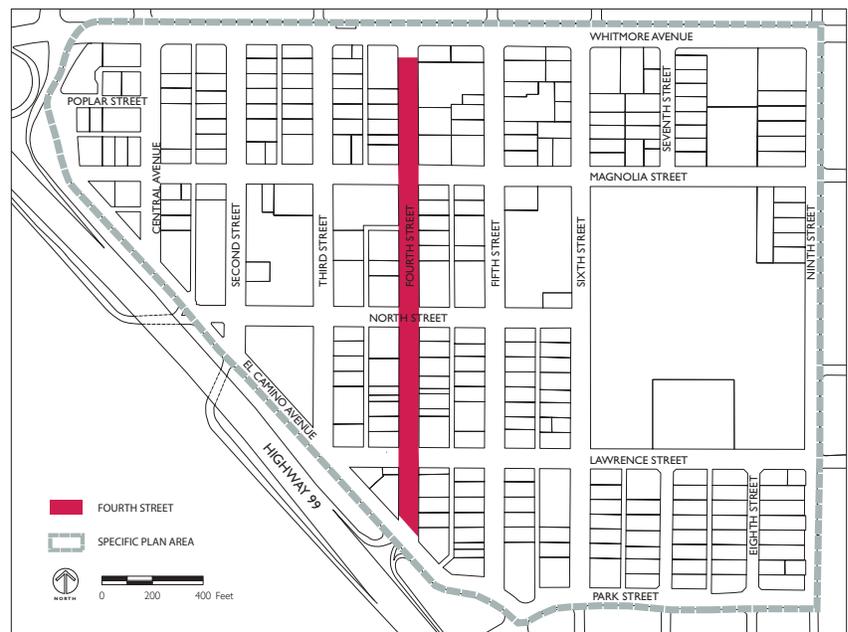
## C. Downtown Streetscape Standards

The following standards focus on the design of the public right-of-way, including sidewalk areas, street landscaping, on-street parking areas and travel lanes. They are intended to facilitate a pedestrian-oriented and active street environment. Recommended street trees were selected from the City of Ceres Master Street Tree List. The standards in this section should be adjusted to accommodate existing special conditions in future detailed streetscape planning processes. Standards are provided for the following streets and public open space types:

- Fourth Street
- Civic Center/Office Streets
- SR 99 Gateway Commercial Streets (Central Avenue)
- Eastern Residential and East-West Connector Streets
- Park Street/Pine Street/El Camino Avenue
- Whitmore Avenue Overlay
- North Street Overlay
- Fifth Street Historic Overlay
- Open Space and Parks
- Alleys

## Fourth Street

Fourth Street is the commercial core and activity center in Downtown. The guidelines below are intended to ensure that improvements within the Fourth Street right-of-way are consistent with the vision in the Specific Plan for this area. The guidelines are focused on providing a comfortable, pedestrian-oriented streetscape environment.



### *Sidewalk and Street Design*

- Sidewalks should be widened to 10 feet and up to 16 feet where possible.
- Where visible, utility wires and poles should be undergrounded.
- Diagonal parking should be provided for the entire length of Fourth Street in Downtown.
- Travel lane widths should not exceed 11 feet along Fourth Street to provide additional room for pedestrian amenities and angled parking.
- Intersection bulbouts should be provided at all street intersections with Fourth Street, building on improvements in the southern portion of the street.
- Intersection bulbouts should incorporate significant landscaping and planting to provide shade and add to the aesthetic variety on Fourth Street.
- Mid-block crossings with special paving should be provided at points where paseos cross the street.

- Consider a special design, such as special paving, for the intersection of North and Fourth Street to highlight the importance of these two streets. Improvements should not impede vehicular flow through the intersection or pedestrian connections. Remove the existing traffic circle. Conceptual plans for the design of this intersection are provided in Chapter Four, Development Concepts.

**Street Trees**

- Climate-appropriate street trees should be provided along Fourth Street to provide shade for pedestrians, assist in stormwater management, buffer pedestrians from vehicular traffic, and provide visual interest on the street.
- The plan recommends consideration of the Evergreen Ash as a street tree standard for Fourth Street.
- Street trees should be provided in a consistent manner for the entire length of Fourth Street.



**Street Lights**

- Street lighting should be provided for both pedestrians and vehicles. Street lighting should be provided at intervals of approximately 30 feet, alternating from one side of the street to the other. Street lighting distance standards are intended to promote a pedestrian-scale and ensure that ample lighting is provided on Fourth Street.
- Street lighting fixture style on Fourth Street should build off of existing street lighting fixtures.
- Street lamps should be oriented toward the ground and designed with “dark sky” technology to help preserve views of the night sky.

**Street Furniture**

- Street furniture should be provided on Fourth Street, including newspaper racks, trash receptacles, benches, public art and other similar elements.
- Pedestrian amenities should be strategically placed to ensure ADA compliance and a clear path of travel for pedestrians, and should not distract or interfere with vehicular circulation.

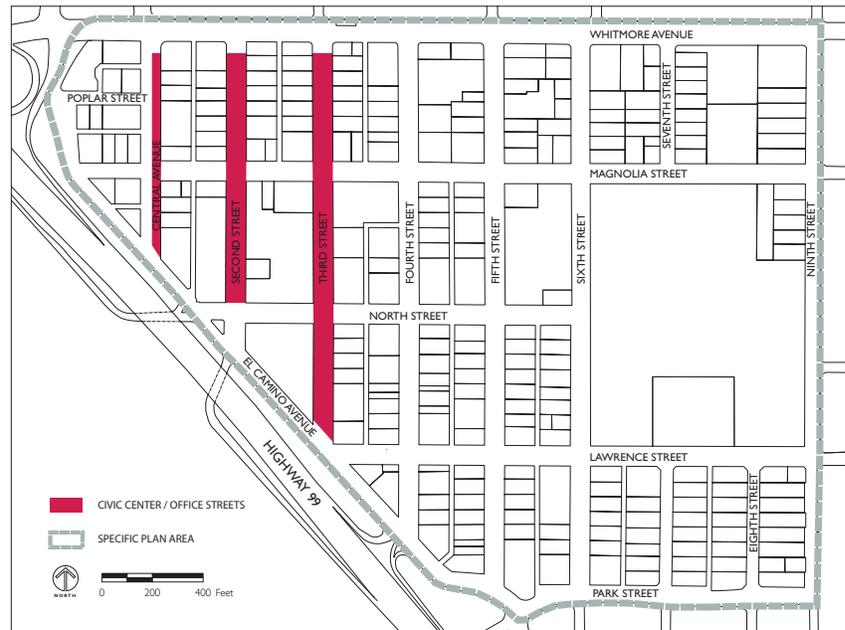
**Bicycle Amenities**

- Bicycle parking facilities should be provided on and near Fourth Street, where possible. Parking facilities should be provided with racks. Larger racks should be provided within bulbouts where there is additional public space. Smaller racks should be placed near the curb on sidewalks where necessary and feasible.



### Civic Center/Office Streets

The following guidelines are specific to the Civic Center/Office district.



#### Sidewalk and Street Design

- Sidewalks should be widened to a minimum of 6 feet, where possible.
- Streetscape improvements along Third Street should not impede entrance or exit from Fire Station #1.
- On-street parking should be provided as angled where possible, but it should be closely coordinated with the access requirements for emergency vehicles associated with the Police building and Fire Station #1.
- Travel lane widths should not exceed 12 feet to provide additional room for pedestrian amenities and angled parking. Travel lanes should still provide for safe circulation of emergency vehicles.
- Mid-block crossings with special paving should be provided at points where paseos cross the street.
- Third Street, between North and Magnolia, should be treated with special landscaping materials to respond to increased pedestrian activity from the civic buildings existing at this location.

**Street Trees**

- Climate-appropriate street trees should be provided along Civic Center/ Office streets to provide shade for pedestrians, assist in stormwater management, buffer pedestrians from vehicular traffic, and provide visual interest on the street.
- The plan recommends consideration of the Scarlet Oak as a street tree standard for Civic Center/Office streets.

**Street Lights**

- Street lighting should be provided for pedestrians and vehicles.
- Street lighting should be provided on Second and Third Streets as needed and in coordination with new development and public improvements.
- Street lamps should be oriented toward the ground and designed with “dark sky” technology to help preserve views of the night sky.

**Street Furniture**

- Pedestrian amenities should be provided on Second and Third Streets, including trash receptacles, benches, public art and other similar elements. These amenities are particularly important around existing and proposed transit stops.
- Pedestrian amenities should be strategically placed to ensure ADA compliance and a clear path of travel for pedestrians, and should not distract or interfere with vehicular circulation.

**Bicycle Amenities**

- Bicycle parking facilities should be provided on and near Second and Third Streets as necessary to augment bicycle parking provided as a part of community buildings, office and other private development. Parking facilities should be provided with racks. Larger racks should be provided within bulbouts where there is additional public space. Smaller racks should be placed near the curb on sidewalks where necessary and feasible.





### SR 99 Gateway Commercial Streets (West Side of Central Avenue)

If parcels are assembled to facilitate a large development in the SR 99 Gateway Commercial District, the following guidelines should be followed. The future of this area should remain flexible and follow the Civic Center/Office Streets guidelines as necessary.

#### *Sidewalk and Street Design*

- Sidewalks should be provided as part of development and at a width of at least 6 feet.
- Streetscape improvements on the west side of Central Avenue should be provided and defined by the private development, but still serve the public and pedestrians using the street for circulation.
- On-street parking should be provided where possible, but should be coordinated with entrances to large commercial parking areas and the traffic lane configuration at Central Avenue and Whitmore Avenue.
- Travel lane widths should not exceed 12 feet to provide additional room for pedestrian amenities and additional on-street public parking for Downtown.



**Street Trees**

- Climate-appropriate street trees should be provided along Central Avenue and internal streets to provide shade for pedestrians, assist in stormwater management, buffer pedestrians from vehicular traffic, and provide visual interest on the street.
- The plan recommends consideration of the Evergreen Ash as a street tree standard for SR99 Gateway Commercial streets.

**Street Lights**

- Street lighting should be provided for pedestrians and vehicles.
- Street lighting should be provided on Central Avenue as needed and in coordination with new development.
- Street lamps should be oriented toward the ground and designed with “dark sky” technology to help preserve views of the night sky.

**Street Furniture**

- Pedestrian amenities should be provided on Central Avenue, including trash receptacles, benches, public art and other similar elements.
- Pedestrian amenities should be strategically placed to ensure ADA compliance and a clear path of travel for pedestrians, and should not distract or interfere with vehicular circulation.

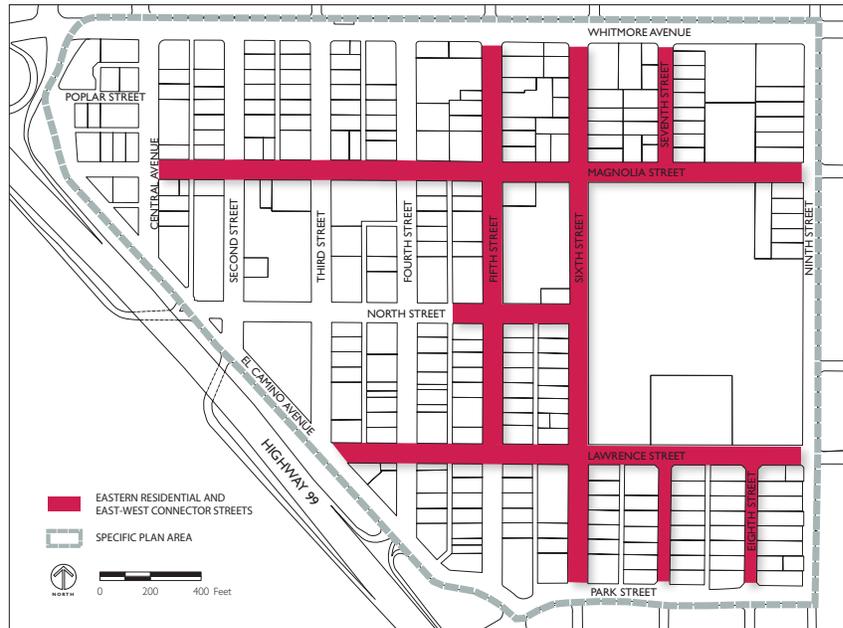
**Bicycle Amenities**

- Bicycle parking facilities should be provided on and near Central Avenue as necessary to augment bicycle parking provided as a part of private development. Parking facilities are most commonly provided with racks. Larger racks should be provided within bulbouts where there is additional public space. Smaller racks should be placed near the curb on sidewalks where necessary and feasible.
- Class III bicycle routes should be striped or signed along Central Avenue within the SR 99 Gateway Commercial district, as designated in the Ceres General Plan.



### Eastern Residential and East-West Connector Streets

The following guidelines are specific to streets in the Eastern Residential district, as well as Lawrence and Magnolia Streets.



#### Sidewalk and Street Design

- Sidewalks should be provided along all Eastern Residential streets at a width of at least 6 feet.
- On-street parking should be provided where possible, but should be coordinated with entrances to residential parking areas.
- Travel lane widths should not exceed 11 feet to provide additional room for pedestrian amenities.
- Mid-block crossings with special paving should be provided at points where paseos cross the street.
- Striped crosswalks should be provided at all intersections where possible, and particularly at intersections that experience heavy student traffic.

**Street Trees**

- Climate-appropriate street trees should be provided along Fifth Street to provide shade for pedestrians, assist in stormwater management, buffer pedestrians from vehicular traffic, and provide visual interest on the street.
- The plan recommends consideration of the Scarlet Oak as a street tree standard for Eastern Residential streets.
- Where possible, trees should be planted in between the sidewalk and street within a planting strip to buffer vehicles from pedestrians.



**Street Lights**

- Street lighting should be provided for pedestrians and vehicles.
- Street lighting should be provided on Eastern Residential streets as needed and in coordination with new development.
- Street lamps should be oriented toward the ground and designed with “dark sky” technology to help preserve views of the night sky.

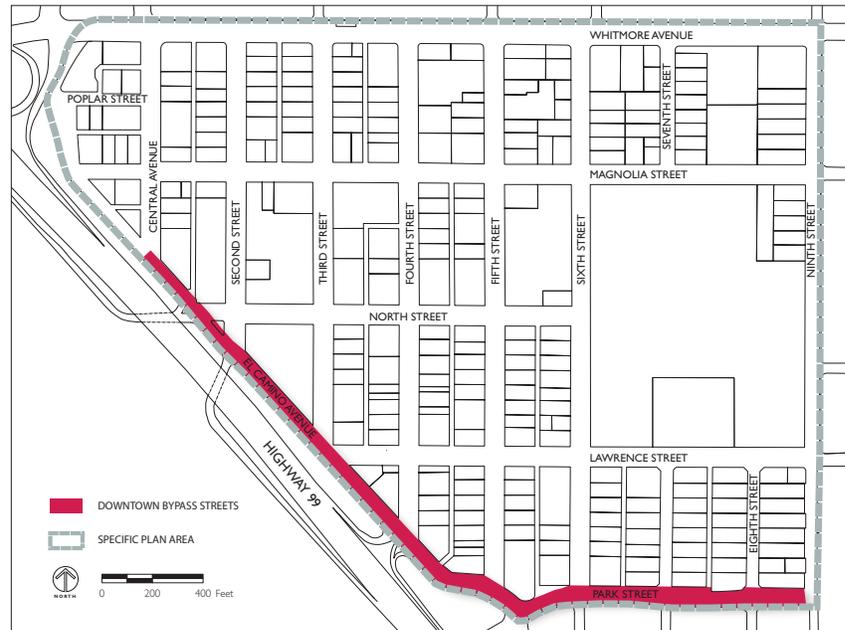
**Bicycle Amenities**

- Class III bicycle routes should be striped or signed along Sixth and Ninth Streets within the Eastern Residential district.



### Downtown “Bypass” Streets

The following guidelines are for the streets that border the edges of Downtown: Park Street, Pine Street and El Camino Avenue.



#### Sidewalk and Street Design

- These streets should be preserved as a vehicular corridor, but enhanced with bicycle improvements.
- Where possible, sidewalks should be provided at a minimum width of 6 feet.

#### Pedestrian Amenities

- Pedestrian amenities should be strategically placed to ensure ADA compliance and a clear path of travel for pedestrians, and should not distract or interfere with vehicular circulation.

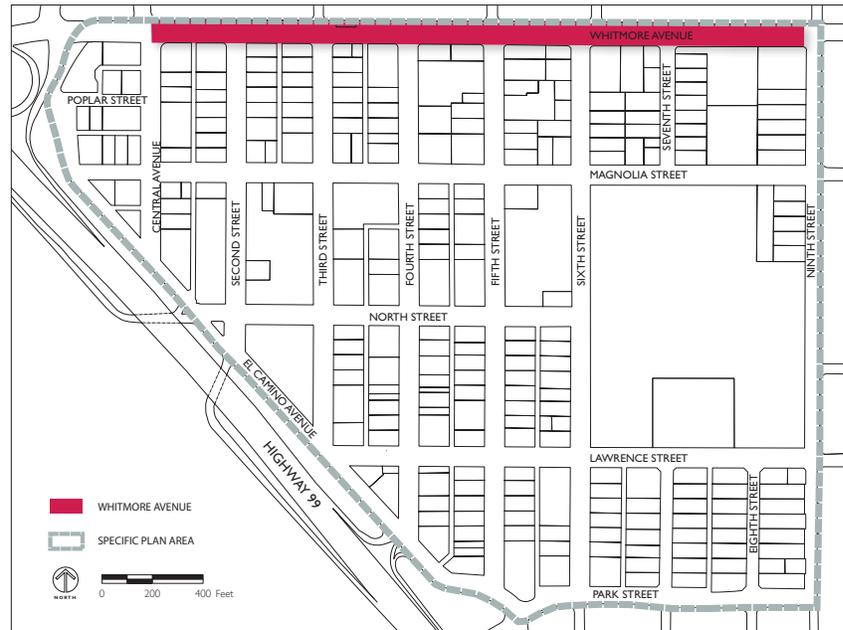
***Bicycle Amenities***

- Class III bicycle routes should be striped or signed along Downtown bypass streets, as designated in the Ceres General Plan.
- Bicycle parking facilities should be provided on and near Whitmore Park. Parking facilities should be provided with racks.



## Whitmore Avenue

The following guidelines are specific to Whitmore Avenue.



### Sidewalk and Street Design

- Sidewalks along Whitmore Avenue should be provided at a minimum width of 6 feet.
- Where visible and where feasible, utility wires and poles should be undergrounded.
- Whitmore Avenue should be reconfigured to include a median with turn pockets at Central Avenue and Third, Fourth and Fifth Streets.
- Travel lane widths should be minimized to provide additional room for pedestrian amenities and a central median. Inside lanes should not exceed 11 feet. Outside lanes should not exceed 12 feet.
- Gateways and wayfinding should be incorporated into medians to signify important entries to Downtown areas.
- Gateway and wayfinding signage should be externally lit to ensure visibility at night.

### Street Trees

- Large (25'-35') canopy street trees should be provided along Whitmore Avenue, including within the median.
- Climate-appropriate street trees should be provided along Whitmore

Avenue to provide shade for pedestrians, assist in stormwater management, buffer pedestrians from vehicular traffic, and provide visual interest on the street. The plan recommends consideration of the Raywood Ash as a street tree standard for Whitmore Avenue.

- Where possible, trees should be planted in between the sidewalk and street within a planting strip to buffer vehicles from pedestrians.

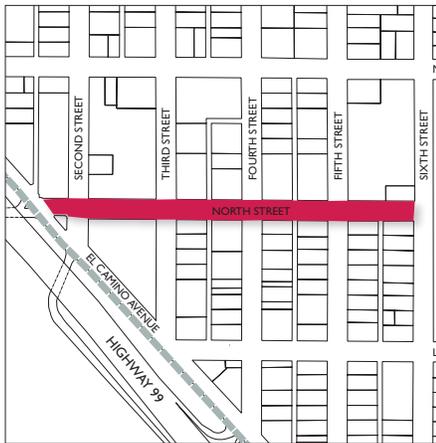


### ***Street Lights***

- Existing cobra head street lighting should be relocated in coordination with utility undergrounding, lane reconfiguration and other improvements to the street.
- Street lamps should be oriented toward the ground and designed with “dark sky” technology to help preserve views of the night sky.
- To the extent feasible, additional pedestrian-scale lighting should be provided along sidewalks for increased pedestrian safety.

### ***Bicycle Amenities***

- Class II bike lanes should be provided on both sides of Whitmore Avenue within Downtown. Bike lanes should be constructed at a minimum width of 5 feet.



## North Street

The following guidelines are specific to North Street.

### *Sidewalk and Street Design*

- Sidewalks should be provided at a minimum width of 8 feet.
- Where visible, utility wires and poles should be undergrounded.
- Sidewalks should be separated from the street with engineered bioswales, where possible. Please see the next page for further details about bioswales and how they function within an urban setting.
- Bioswales should be a minimum width of 4 feet.
- Grasses and shrubs, such as California Grey Rush and Monkey Flower, and smaller trees should be provided within bioswales.
- Sidewalk construction should provide a 3 to 4 foot paved area between on-street parking and bioswales to allow people to exit vehicles safely.
- Paved areas allowing for vehicle exiting should be bridged over bioswales to connect to sidewalks at regular intervals.
- Travel lane widths should not exceed 12 feet to provide additional room for pedestrian amenities, landscaping and stormwater management infrastructure.

### *Street Trees*

- Large (25'-35') canopy street trees should be provided along North Street to implement the North Street Green Connection concept described in Chapter Four of this Specific Plan.
- Climate-appropriate street trees should be provided along North Street to provide shade for pedestrians, assist in stormwater management, buffer pedestrians from vehicular traffic, and provide visual interest on the street. The plan recommends consideration of the London Plane tree as a street tree standard for North Street, as it is an excellent large-canopy tree that can tolerate wet roots during stormwater retention.
- Where no bioswale is provided, trees should be planted in between the sidewalk and street within a planting strip to buffer vehicles from pedestrians.

### *Street Lights*

- Street lighting should be provided for pedestrians and vehicles. Street lighting should be provided at intervals of at least 30 feet, alternating from one side of the street to the other. Street lighting distance standards are intended to promote a pedestrian-scale environment and ensure that ample lighting is provided on North Street.
- Street lighting fixture style on North Street should build off of existing street lighting fixtures.

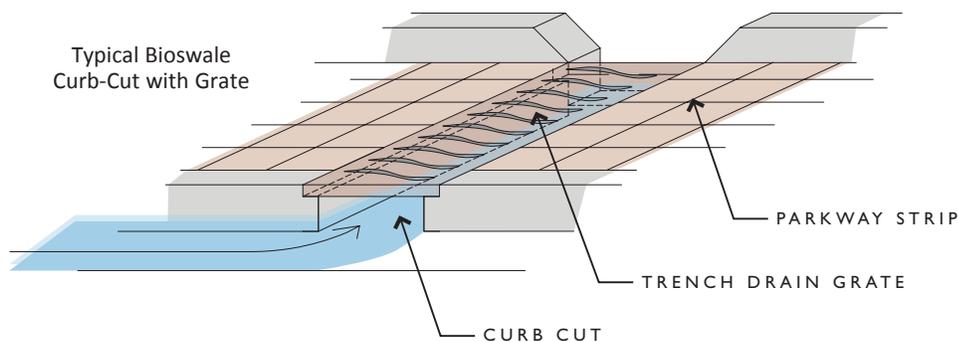
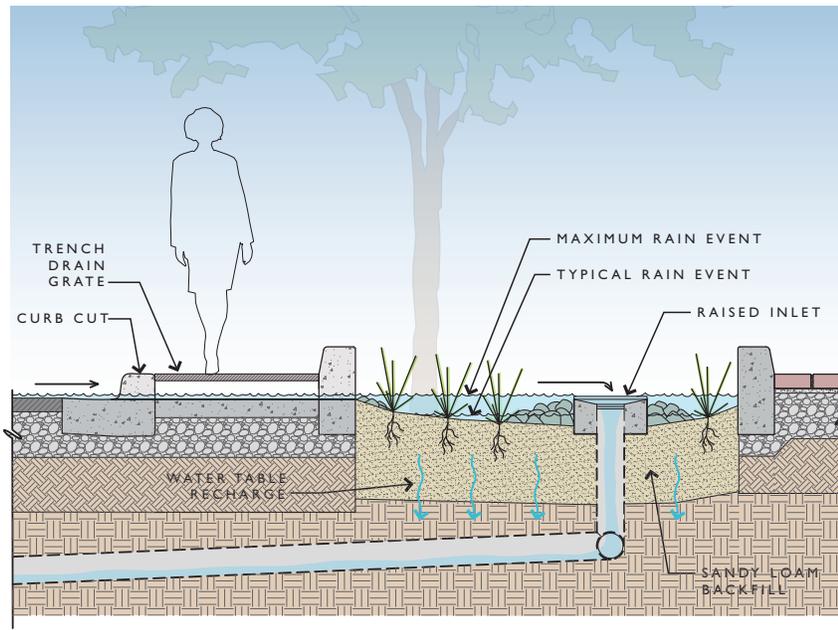
- Street lamps should be oriented toward the ground and designed with “dark sky” technology to help preserve views of the night sky.

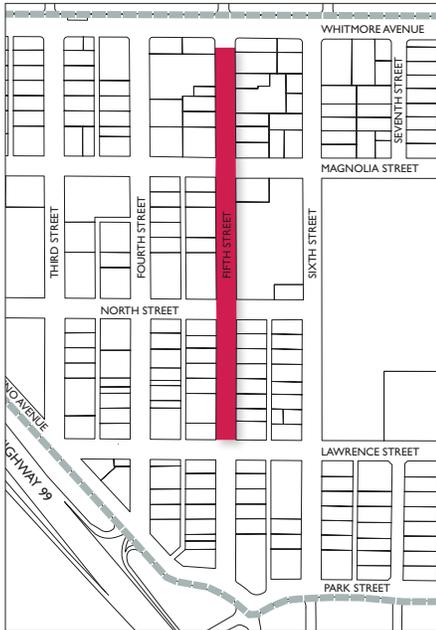
**Street Furniture**

- Pedestrian amenities should be provided on North Street, including trash receptacles, benches, public art and other similar elements.
- Pedestrian amenities should be strategically placed to ensure ADA compliance and a clear path of travel for pedestrians, and should not distract or interfere with vehicular circulation.



Typical Bioswale





### **Fifth Street Historic Overlay**

The following guidelines are specific to Fifth Street, between Lawrence Street and Whitmore Avenue.

#### ***Sidewalk and Street Design***

- New sidewalks should be provided at a minimum width of 6 feet.
- Where visible, utility wires and poles should be undergrounded.
- Sidewalks should be separated from the street with engineered bioswales where possible.
- Bioswales should be a minimum width of 4 feet.
- Grasses and shrubs, such as California Grey Rush and Monkey Flower, and smaller trees should be provided within bioswales.
- Sidewalk construction should provide a 3 to 4 foot paved area between on-street parking and bioswales to allow people to exit vehicles safely.
- Paved areas allowing for vehicle exiting should be bridged over bioswales to connect to sidewalks at regular intervals.
- Travel lane widths should be minimized to provide additional room for pedestrian amenities.
- Sidewalks should be provided in an organic, curvilinear pattern consistent with the improvements identified for this street. Additional information about the potential reconfiguration of this section of Fifth Street is provided in Chapter 8, Circulation.
- Varying setbacks and pedestrian areas should be provided, building off of the existing variation in development setbacks in this area.

#### ***Street Trees***

- Existing mature street trees should be incorporated into streetscape improvements wherever possible.
- New street trees should be provided along Fifth Street, both in bioswales and in landscaped portions of the right-of-way without bioswales.
- Climate-appropriate street trees should be provided along Fifth Street to provide shade for pedestrians, assist in stormwater management, buffer pedestrians from vehicular traffic, and provide visual interest on the street. The plan recommends consideration of the London Plane tree as a street tree standard for North Street, as it is an excellent large-canopy tree that can tolerate wet roots during stormwater retention.
- Where no bioswale is provided, trees should be planted in between the sidewalk and street within a planting strip to buffer vehicles from pedestrians.

**Street Lights**

- Street lighting should incorporate lighting for pedestrians and vehicles.
- Street lamps should be oriented toward the ground and designed with “dark sky” technology to help preserve views of the night sky.

**Street Furniture**

- Pedestrian amenities should be provided on Fifth Street, including benches, public art, historic markers and signage, extended landscaped areas, and other similar elements. New street furniture and pedestrian scale elements should be consistent with the residential and historic character of this street.
- Pedestrian amenities should be strategically placed to ensure ADA compliance and a clear path of travel for pedestrians, and should not distract or interfere with vehicular circulation.



## Open Space and Parks

This section provides design guidance for the provision of public open space within Downtown. Open space areas include larger community parks, smaller neighborhood or pocket parks, plazas, and paseos. Provision of attractive and accessible open space is a primary goal of the Specific Plan and these guidelines are intended to help achieve that goal.

### Community Parks

- Locate community parks where they are accessible from all parts of the Downtown and are accessible to visitors citywide.
- Activate community parks by locating complementary uses adjacent to them, such as housing that faces and engages the park.

### Neighborhood and Pocket Parks

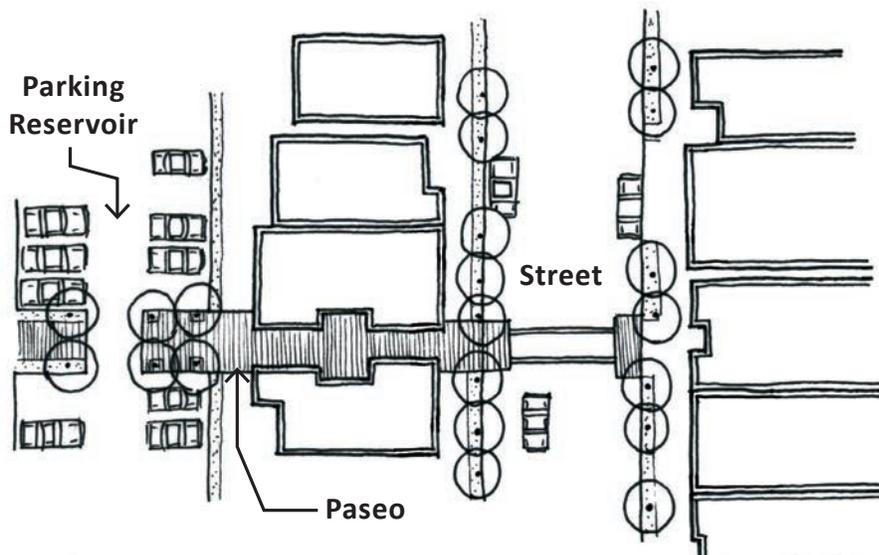
- Distribute neighborhood parks throughout Downtown, so that all neighborhoods within the area are served.
- For safety, design neighborhood parks to maximize visibility from the street and adjacent homes.
- Site smaller neighborhood parks in areas of Downtown that lack access to open space.
- Use neighborhood parks to provide small-scale amenities, such as playgrounds, where needed.
- Encourage the development and stewardship of neighborhood parks through public and private partnerships.
- Ensure that neighborhood parks are visible and accessible from public sidewalks, particularly in the eastern residential areas of Downtown.

### Plazas

- Locate plazas in places that attract large amounts of foot traffic, so that they are active and well used.
- Limit the size of plazas to approximately 150 to 300 square feet per person, based on the typical number of people that will occupy the space when it is being used.
- Ensure that buildings open onto plazas and their uses expand into the open space.
- Provide a focal point for pedestrian gathering in the center of the plaza.
- Locate plazas where they are visible from the street.
- Provide clear transitions between plazas.
- Develop plazas to maximize circulation opportunities between adjacent buildings and to highlight important civic spaces.

**Paseos**

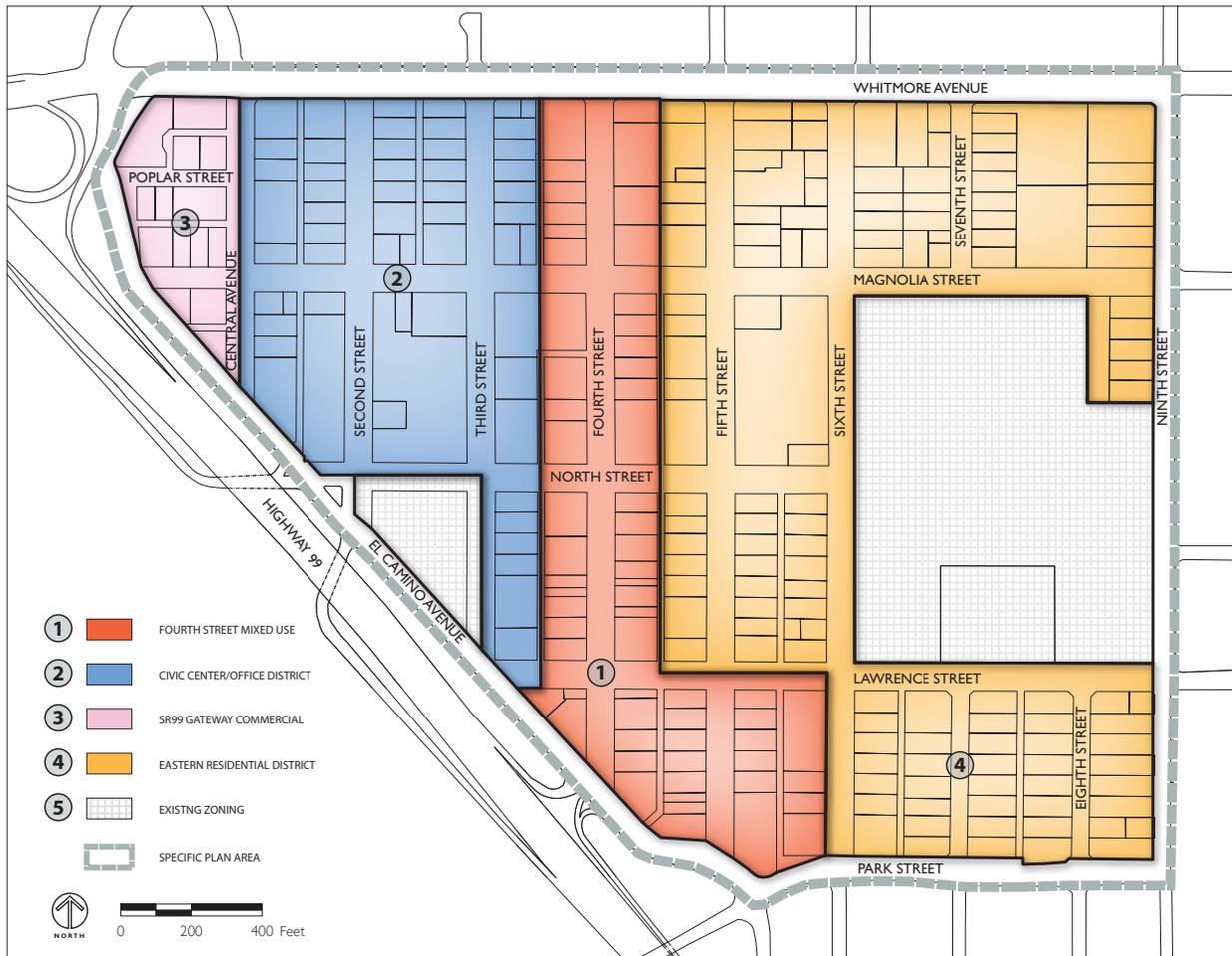
- Paseos should be provided at a minimum width of 15 feet.
- Paseos should be dynamic spaces that can be adapted to serve different activities.
- Wherever possible, paseos should be designed to be integrated with nearby development and open spaces.
- Paseos should provide significant pedestrian amenities, including trash receptacles, benches, public art and other similar elements.
- Shade trees should be provided within paseos.
- Paseos should be paved with special and recognizable paving, such as permeable concrete pavers, brick, decorative concrete, or some other pavement pattern, and be well connected throughout Downtown.
- Paseos should serve as public gathering spaces and pedestrian circulation connections, and should be designed as such.
- Awnings or canopies are encouraged for seasonal use so as to provide summer shade. These elements could be removed in the winter to extend service life. These elements should be of materials that are consistent with or complementary to paseo materials.



### D. Design Guidelines

This section provides design guidance for private development. These guidelines will be used during the entitlement process by the Planning Commission and/or City Council for all new buildings or façade improvements. Guidelines are provided for each of the four designated districts to ensure appropriate development in Downtown: the Fourth Street Mixed Use District, the Civic Center/Office District, the SR 99 Gateway Commercial District, and the Eastern Residential District. Guidelines for landscape design, lighting and signage are consistent throughout Downtown, rather than districts based, and are included at the end of the Design Guidelines.

FIGURE A-1 District Boundaries



### **Fourth Street Mixed Use District**

This Fourth Street Mixed Use District will be the commercial heart of Downtown. It is very important that all new development, including renovations of existing structures, present a cohesive and attractive frontage on Fourth Street.

### **Architectural Style and Character**

New construction and façade improvements in the Fourth Street Mixed Use District should build on the existing “Main Street” character of Fourth Street. This character, also called 20<sup>th</sup> Century Commercial, consists of continuous street frontage (no side yards between buildings) with glazed store fronts on the ground floor and solid walls with individual window openings at upper stories. The existing Odd Fellows building is an example of this style. Materials should look substantial and can vary from brick to stone to stucco. Detailing can draw from any one of many styles including Neo-classical, Italianate, Craftsman, Art Deco or Contemporary. However, individual buildings should be consistent within one style and not mix styles.

**Site Planning** - These guidelines are intended to ensure development creates an attractive commercial environment that works well for merchants, visitors and residents.

### **Building Location and Orientation**

- Parking shall be provided at the rear of the building. This will create a continuous street frontage that is pleasant to walk along. If parking is provided at the side of the building, it shall include a “street edge” of landscaping, walls or trellises. The “street edge” is the general linear progression of building façades along a street, particularly when building frontages are built to the property line.
- Main façades with entrance doors and windows should front the primary street.
- The location of site uses should be coordinated with adjoining properties and consider factors such as noise, light intrusion, invasion of privacy and traffic.
- Owners of adjoining properties are strongly encouraged to develop shared facilities, such as driveways, parking areas, pedestrian plazas and walkways.



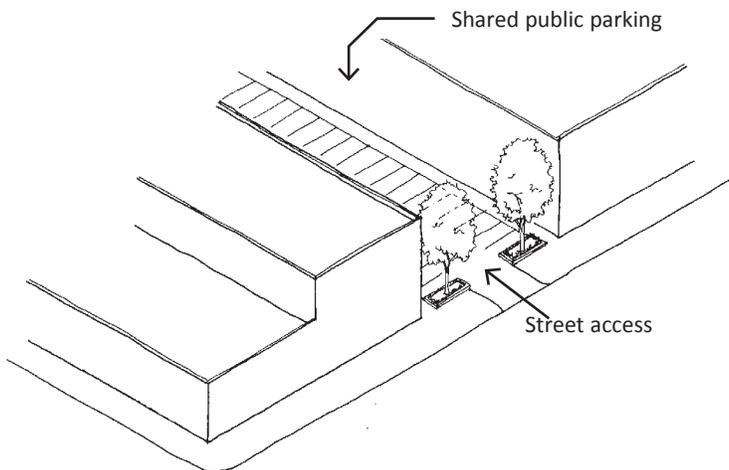
**Fourth Street Mixed Use District**

**Gateway Parcels**

- New development should provide a taller tower feature or vertical component at the corner parcels where Fourth Street intersects Whitmore and El Camino Avenues. Building features should be designed to help accentuate entries to Fourth Street and clearly differentiate Fourth Street from other streets in Downtown.

**Corner Sites**

- At street corners, new development should either be sited on the corner property lines, if no public utility easements exist, or set back from the corner only if a public open space is provided.
- Buildings located on corners should include special design and architectural features that help to anchor the intersection.



**Development Along Alleys**

- Shared public parking within alleys shall be provided for all development.
- Trash enclosures, service areas, utility meters, and mechanical and electrical equipment should be located in alleys and screened from public view with landscaping or well-designed enclosures.

***Sustainable Site Development***

- Existing buildings should be reused and incorporated into new development, if possible. If reuse or incorporation of existing buildings is not possible, then building materials should be recycled.
- Solar access should be considered when site planning. Buildings should be oriented to account for winter and summer sun to maximize energy efficiency and to reduce shading on neighboring properties and public spaces.
- Non-vehicular modes of transportation should be encouraged by providing no more than the minimum number of vehicle parking spaces, creating attractive pedestrian environments, and providing bicycle parking.
- Permeable paving materials should be used whenever possible. The primary purpose of permeable paving is to manage stormwater drainage on site and reduce the impact of new development on public storm drainage infrastructure.



Trees provide areas of shade for a corner plaza in the summer.



Deciduous trees allow for winter sun to penetrate into the plaza space.

**Fourth Street Mixed Use District**

***Pedestrian Orientation***

- All commercial mixed use area projects shall emphasize pedestrian orientation by creating attractive pedestrian spaces that utilize features such as plazas, interior walkways and paseos.
- Outdoor pedestrian space shall be landscaped and include appropriate street furniture to facilitate pedestrian activity.
- Attractive well-marked pedestrian links between parking and buildings shall be provided.



***Open Space***

- Open space, plazas, and paseos should be developed to maximize circulation opportunities between adjacent buildings. Seating areas should be provided and coordinated with shading, landscaping, and lighting.
- Private residential open space areas should be configured and designed to ensure privacy for residential uses while also providing linkages to the public open space components of the project.

**Parking Area Design**

- Parking in the Fourth Street Mixed Use District shall be provided behind or within buildings. Shared public parking shall be provided at all alley frontages.
- If parking is provided at the side of buildings, these areas shall be buffered by landscaping, low walls and fencing. For security purposes, openings shall be incorporated into the design of buffers to provide views into the site.
- Landowners should be encouraged to enter into shared parking agreements that allow uses with different peak hours of operation to utilize off-street parking facilities provided by another building or use.
- All outdoor parking areas should be divided into smaller units to decrease visual impacts associated with large expanses of pavement and vehicles, and to facilitate safe and efficient pedestrian movement between parking and residential and commercial development.
- Access to parking from Fourth Street is strongly discouraged.
- All new projects should provide bicycle racks that are located close to the buildings and do not impede pedestrian or auto circulation.



**Fourth Street Mixed Use District**

**Building Design** - Building design guidelines are intended to ensure that development of all sizes relates to the human scale, facilitates opportunities for pedestrian activity on adjoining public streets, and contributes to the character of the Fourth Street Mixed Use District.

**Street Wall Design**

- Buildings on larger lots shall be broken down in scale to create a small-scale street frontage rhythm, with building storefront widths of approximately 30 to 50 feet to match the existing pattern of older commercial development in Downtown. This rhythm shall be expressed by vertical divisions extending the height of the building.



Examples of building frontage broken up every 30 to 50 feet.

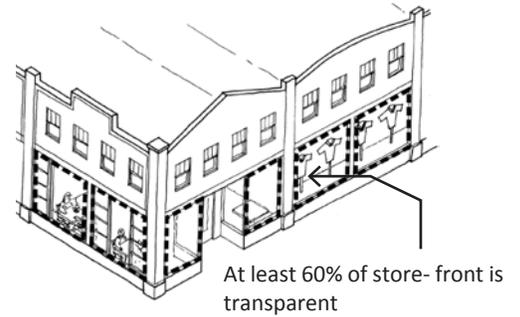


- It is recommended that vertical divisions express the structural system of the building.
- All street walls shall emphasize three-dimensional detailing such as cornices, recessed windows, and reveals to cast shadows, create visual interest on the façade, and provide human scale.
- Façades without openings shall be avoided on Fourth Street.
- Entrances should incorporate one or more of the following treatments:
  1. A taller mass above, such as a tower or volume.
  2. A special architectural elements, such as columns, overhanging roofs, trellises and awnings.
  3. A recessed entry or recessed bay in the façade.

**Fourth Street Mixed Use District**

**Ground Floor Frontages**

- Ground-floor façades should be designed to give individual identity to each retail establishment.
- All ground floor frontages on Fourth Street shall have a minimum of 60 percent transparency. This should be achieved with glass storefronts, entries or display windows.
- Entries to ground-floor retail areas and main building entries or entries to upper story uses shall occur from primary streets.
- Main building entries or entries to upper story uses should be recessed into entry bays to create transitional spaces between the street and buildings.
- Where unique use or occupancy requirements preclude the addition of windows, such as theaters or parking structures, exterior walls shall be painted with murals designed to provide architectural relief, or shall be screened by landscaping and pedestrian amenities, such as trellises, benches, sculptures or shade structures.
- Awnings and canopies over building entries shall be integrated into the design of the building, including colors and detailing. They should be provided over each storefront for buildings with multiple storefronts, and should be located within individual structural bays.
- Backlighting of transparent or translucent awnings shall not be allowed.

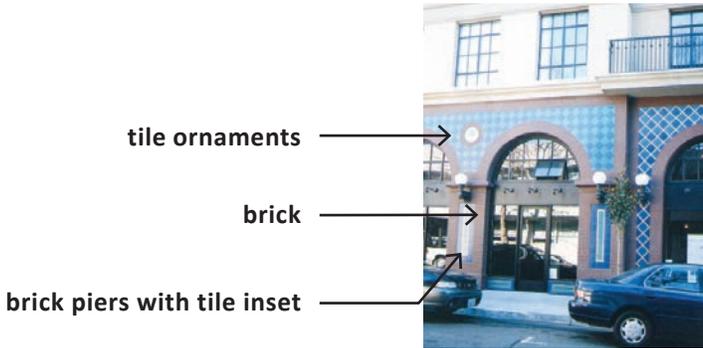
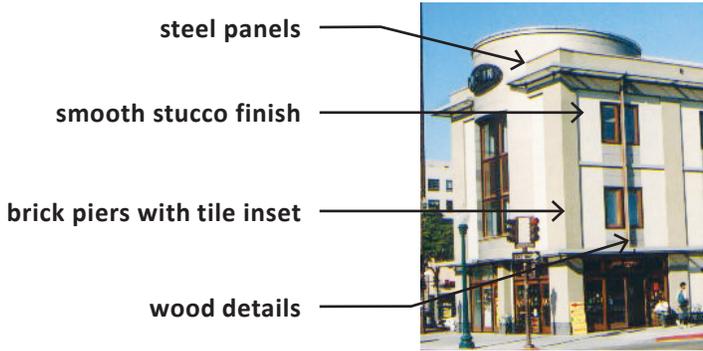
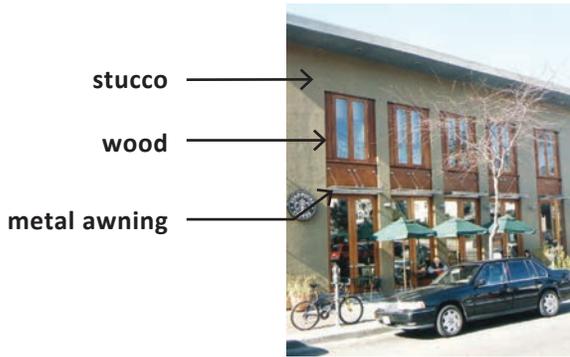


**Windows**

- Upper story windows shall be detailed with well-proportioned architectural elements, such as sills, recesses and lintels.
- Operable windows shall be used.
- Clear glass shall be used.
- Windows should maintain consistency in shape and location across the street wall.
- Non-reflective coatings, low-emissivity glass, and external shade devices should be used for heat and glare control.



Fourth Street Mixed Use District



**Building Materials**

- Materials should be chosen and detailed to respect the climate and traditions of the Central Valley of California. In the Fourth Street Mixed Use District the primary material should have a solid character. These may include brick, tile, stone or stucco. Whatever the material, the wall surfaces should communicate solidness by recessing windows and other openings a minimum of four inches. Accent materials (brackets, sills, corbels) may be wood or metal.
- Genuine materials should be utilized rather than simulated materials. Where simulated materials are used, they should keep with the character and properties of the material being simulated.
- Artwork should be incorporated into building design at the ground level.
- Lively exterior colors are appropriate for Fourth Street.

***Roofs and Parapets***

- All buildings shall provide roof cornices or parapets in order to delineate a strong cap to the street wall.
- The form, color and texture of parapets shall be an integral component of the building design.
- Roofs should be proportionate to building mass and incorporate cornices, eaves and overhangs.
- All roof-mounted mechanical, electrical and external communication equipment, such as satellite dishes and microwave towers, shall be screened from public view and architecturally integrated into the building design.
- Landscape design, lighting and signage guidelines are found at the end of these design guidelines.



### **Civic Center/Office District**

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The Civic Center/Office District will be the civic and office employment center of Downtown Ceres.



### **Architectural Style and Character**

The existing architectural style of the Civic Center/Office District is eclectic. Some of the existing civic buildings are modest “modern” buildings from the 1960s and many other buildings in this district are houses from varying eras. New office development in this district will generally be free standing (with side yards) and should be in a simple contemporary style. The recent TID building facing Whitmore Park is a good example. Roofs may be sloping or flat with parapets. Quality of materials should be high and attention should be paid to landscaping. New civic buildings, such as a relocated library or city hall, should have a more monumental character with a substantial public entry as a focal point. The new Community Center on Fourth Street is an example of this.

**Site Planning** - These guidelines are intended to ensure civic, office and mixed use development creates an attractive environment that works well for employers, civic departments and activities, and residents.

### **Building Location and Orientation**

- Parking shall be provided at the rear of the building. This will create a continuous street frontage that is pleasant to walk along. If parking is provided at the side of the building, it shall include a “street edge” of landscaping, walls or trellises.
- Main façades with entrance doors and windows should front the primary street.
- The location of site uses should be coordinated with adjoining properties and consider factors such as noise, light intrusion, invasion of privacy and traffic.
- Owners of adjoining properties are strongly encouraged to develop shared facilities, such as driveways, parking areas, pedestrian plazas and walkways.
- Any new Civic Center development should consider a shared-use public parking area or structure that can be used by employees during weekdays and by visitors to Downtown in the evening or on weekends.



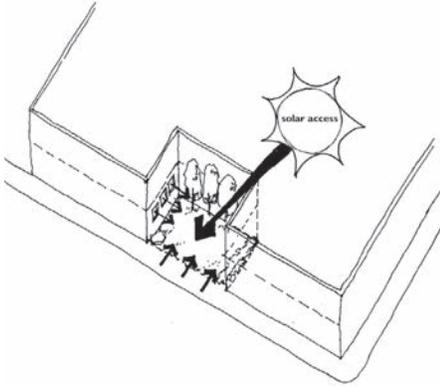
**Corner Sites**

- At street corners, new development should either be sited on the corner property lines, if no public utility easements exist, or set back from the corner only if a public open space is provided.
- Buildings located on corners should include special design and architectural features that help to anchor the intersection.
- Civic Center buildings such as a new city hall or library should be considered for prominent corner sites in Downtown.

**Development Along Alleys**

- Shared public parking within alleys should be provided for all development where feasible.
- Offices, entries, and outside break spaces facing alleys are encouraged, and will provide “eyes on the street” for security.
- Trash enclosures, service areas, utility meters, and mechanical and electrical equipment should be located on alleys and screened from public view.

Civic Center/Office District



**Sustainable Site Development**

- Existing buildings should be reused and incorporated into new development, if possible. If reuse or incorporation of existing buildings is not possible, then building materials should be recycled.
- Solar access should be considered when site planning. Buildings should be oriented to account for winter and summer sun to maximize energy efficiency and to reduce shading on neighboring properties and public spaces.
- Non-vehicular modes of transportation should be encouraged by providing no more than the minimum number of vehicle parking spaces, creating attractive pedestrian environments and providing bicycle parking.
- Permeable paving materials should be used whenever possible. The primary purpose of permeable paving is to manage stormwater drainage on site and reduce the impact of new development on public storm drainage infrastructure.

**Pedestrian Orientation**

- New civic and office projects shall emphasize pedestrian orientation by creating attractive pedestrian spaces that utilize features such as plazas, interior walkways and paseos.
- Outdoor pedestrian space shall be landscaped and include appropriate street furniture to facilitate pedestrian activity.
- Attractive well-marked pedestrian links between parking and buildings shall be provided.



**Open Space**

- Open space, plazas, and paseos should be developed to maximize circulation opportunities between adjacent buildings. Seating areas should be provided and coordinated with shading, landscaping, and lighting.

**Parking Area Design**

- Parking in the Civic Center/Office District shall be provided behind or within buildings, or on public lots. Shared public parking shall be provided at all alley frontages.
- If parking is provided at the side of buildings, these areas shall be buffered by landscaping, low walls and fencing. For security purposes, openings shall be incorporated into the design of buffers to provide views into the site.



Parking buffered by landscaping

- Landowners should be encouraged to enter into shared parking agreements that allow uses with different peak hours of operation to utilize off-street parking facilities provided by another building or use.
- All outdoor parking areas should be divided into smaller units to decrease visual impacts associated with large expanses of pavement and vehicles, and to facilitate safe and efficient pedestrian movement between parking and residential and commercial development.
- All new projects should provide bicycle racks that are located close to the buildings and do not impede pedestrian or auto circulation.

## Civic Center/Office District



**Building Design** - Building design guidelines are intended to ensure that development of all sizes relates to the human scale, facilitates opportunities for pedestrian activity on adjoining public streets, and contributes to the character of the Civic Center/Office District.

### **Street Wall Design**

- Buildings on larger lots shall be broken down in scale to create a small-scale street frontage rhythm, with building storefront widths of approximately 30 to 50 feet to match the existing pattern of older commercial development in Downtown. This rhythm shall be expressed by vertical divisions extending the height of the building.
- It is recommended that vertical divisions express the structural system of the building.
- All street walls shall emphasize three-dimensional detailing such as cornices, recessed windows, and reveals to cast shadows, create visual interest on the façade, and provide human scale.
- Entrances should incorporate one or more of the following treatments:
  1. A taller mass above, such as a tower or volume.
  2. A special architectural elements, such as columns, overhanging roofs, trellises and awnings.
  3. A recessed entry or recessed bay in the façade.



### **Ground Floor Frontages**

- Ground-floor façades should be designed to give interest to pedestrians and visitors.
- Main building entries or entries to upper story uses shall occur from primary streets, and should be recessed into entry bays to create transitional spaces between the street and buildings.
- Where unique use or occupancy requirements preclude the provision of ground floor windows, such as parking structures, exterior walls shall be painted with murals or other artwork designed to provide architectural relief, or shall be screened by landscaping and pedestrian amenities, such as trellises, benches, sculpture or shade structures.
- Awnings and canopies over building entries shall be integrated into the design of the building, including colors and detailing.
- Backlighting of transparent or translucent awnings shall not be allowed.

**Windows**

- Upper story windows shall be detailed with well-proportioned architectural elements, such as sills, recesses and lintels.
- Operable windows shall be used.
- Clear glass shall be used.
- Non-reflective coatings, low-emissivity glass, and external shade devices should be used for heat and glare control.

**Building Materials**

- Materials should be chosen and detailed to respect the climate and traditions of the Central Valley of California. These may include wood siding, sheet metal, well-detailed stucco, tile and stone.
- Genuine materials should be utilized rather than simulated materials. Where simulated materials are used, they should keep with the character and properties of the material being simulated.
- Artwork should be incorporated into building design at the ground level.

**Roofs and Parapets**

- Roofs should be proportionate to building mass and incorporate cornices, eaves and overhangs.
- All roof-mounted mechanical, electrical and external communication equipment, such as satellite dishes and microwave towers, shall be screened from public view and architecturally integrated into the building design.
- Notable civic buildings, such as a new city hall or library, should consider a signature roof element such as a contemporary tower or dome to signify the civic use.
- Landscape design, lighting and signage guidelines are found at the end of these design guidelines.



## **SR 99 Gateway Commercial District**

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This section provides design guidelines for regional commercial and office development, such as the type of development proposed for the SR 99 Gateway Commercial District.

### **Architectural Style and Character**

The SR 99 Gateway Commercial District does not have an existing character to draw upon. Because this district is the farthest from the commercial and historic core of Downtown, and because of the fast pace of change in styles of roadside commercial architecture, the architectural style for new buildings in this district should be flexible. The overriding consideration for new buildings should be the following: from the State Route 99 side, they should communicate a quality and solid character commensurate with the historic Downtown Ceres; and from the Downtown side, they should have an appropriate scale that relates to the block sizes and building frontages of Downtown.

**Site Planning** - Site planning guidelines are intended to ensure regional commercial development contributes to an attractive Downtown that is comfortable for residents and visitors.

### **Building Location and Orientation**

- In Downtown Ceres, even larger commercial/office projects that would be appropriate in the SR 99 Gateway Commercial District should be oriented to public streets, with buildings placed at or near the property line along public streets, if no public utility easements exist, and parking behind.
- The placement and orientation of the building on the site should facilitate access to major streets and thoroughfares.
- Loading docks, truck doors and service areas shall not be located between the primary building and the primary street. Any outdoor storage areas containing materials, supplies or equipment, including heavy trucks and trailers, should be screened from public view.

### **Open Space**

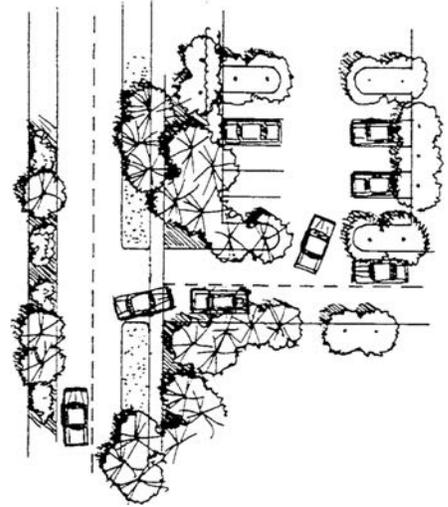
- Publicly-accessible plazas and open spaces shall be landscaped and incorporate high quality paving materials, such as stone, concrete, tile, pavers or brick.
- Paving, planting and other landscape materials shall be coordinated with the design of the building and site.



**Parking**

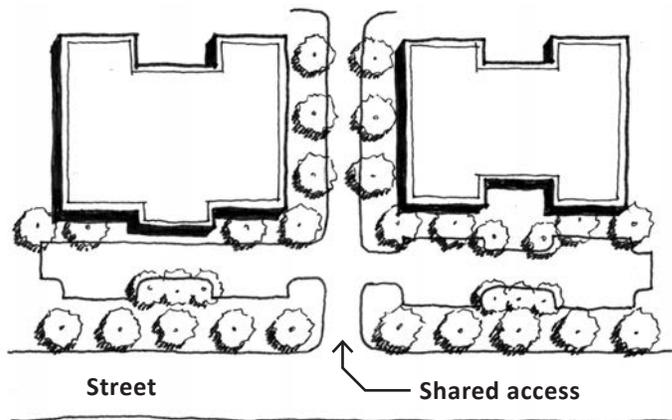
- In order to reduce public views of parking areas, a significant amount of a development’s parking area should be located beside or behind the building to add to the pedestrian environment on Downtown streets. Parking areas may be provided adjacent to State Route 99 to facilitate pedestrian environments on Downtown streets and to serve as buffers between new development and the highway.
- All outdoor parking areas should be divided into smaller units to decrease visual impacts associated with large expanses of pavement and vehicles, and to facilitate safe and efficient pedestrian movement between parking and structures.
- Surface parking areas facing the public street shall be buffered by berming or landscaping.
- For security purposes, openings should be incorporated into the landscape design to provide clear views into the site.

Smaller parking area with landscaped buffer



**Access Drives**

- Access driveways shall be sufficient in number to provide safe and efficient movement of traffic to and from a site; they should, however, be kept to a minimum.
- Common access drives are encouraged for adjacent lots to reduce the number of access points onto the main roadway.
- Multiple-lot office development should provide access to individual lots from an internal street system rather than create additional driveways along public street frontages.



**Internal Circulation**

- On-site pathways should be provided for pedestrians and bicyclists and should provide connections between building entries and public sidewalks.
- Large parking areas associated with commercial development should include at least one separated pedestrian pathway through the parking area to the main entrance. Pedestrian pathways should be provided in parking areas that require pedestrians to walk more than 60 feet to a building entrance within a single development.
- Pedestrian walkways and spaces should include elements such as special paving materials, raised curbs, trellis structures, landscaping, pedestrian lighting, seating and trash receptacles.



**Building Design** - Building design guidelines are intended to ensure that development relates to the human scale, facilitates opportunities for pedestrian activity on adjoining public streets, and contributes to the character of Downtown. High-quality building design is integral to creating a pedestrian-friendly environment and a unique Downtown, and will help draw in new passers-by on State Route 99. It should be noted that larger building floor-plates may be appropriate in this district, particularly to support large, single developments and to respond to traffic on State Route 99.

**Building Design**

- Larger commercial buildings shall be broken down in scale to create a smaller-scale rhythm. This rhythm shall be expressed by vertical divisions extending the height of the building.
- It is recommended that vertical divisions express the structural system of the building.
- All buildings shall emphasize three-dimensional detailing such as cornices, recessed windows, and reveals to cast shadows, create visual interest on the façade, and provide human scale.
- Entrances should incorporate one or more of the following treatments:
  1. A taller mass above, such as a tower or volume.
  2. A special architectural elements, such as columns, overhanging roofs, trellises and awnings.
  3. A recessed entry or recessed bay in the façade.

**Ground Floor Frontages**

- Ground-floor façades along streets should be designed to give interest to pedestrians and visitors.
- Main building entries or entries to upper story uses shall occur from primary streets, and should be recessed into entry bays to create transitional spaces between the street and buildings.
- Where unique use or occupancy requirements preclude the provision of ground floor windows, such as service areas or parking structures, exterior walls shall be painted with murals or other artwork, designed to provide architectural relief, or shall be screened by landscaping and pedestrian amenities, such as trellises, benches, sculpture or shade structures.

**Windows**

- Upper story windows shall be detailed with well-proportioned architectural elements, such as sills, recesses and lintels.
- Operable windows shall be used.
- Clear glass shall be used.
- Non-reflective coatings, low-emissivity glass, and external shade devices should be used for heat and glare control.

**Building Materials**

- Materials should be chosen and detailed to respect the climate and traditions of the Central Valley of California. These may include wood siding, sheet metal, well-detailed stucco, tile and stone.
- Use of quality materials should be used on all visible facades of the building, not just the front of the building.
- Genuine materials should be utilized, rather than simulated materials. Where simulated materials are used, they should keep with the character and properties of the material being simulated.
- Artwork should be incorporated into building design at the ground level.

**Roofs and Parapets**

- Roofs should be proportionate to building mass and incorporate cornices, eaves and overhangs.
- All roof-mounted mechanical, electrical and external communication equipment, such as satellite dishes and microwave towers, shall be screened from public view and architecturally integrated into the building design.
- Landscape design, lighting and signage guidelines are found at the end of these design guidelines.

## **Eastern Residential District**

The Eastern Residential District will accommodate a variety of housing, including single-family homes, townhomes and apartments.



### **Architectural Style and Character**

The existing architectural character in the Eastern Residential District dates from the late 1800s to relatively recent times. This district has several historic properties, most prominently the Whitmore House and Museum, and the Whitmore Mansion. For this reason, new development and renovations of existing buildings in this district should reflect the historic residential architectural styles of the late 19th and early 20th centuries. These styles include Farmhouse Victorian, Queen Anne, Eastlake, and Craftsman. They are characterized by the use of vertical massing, gabled roofs, decorative trim at roof eaves and windows, painted wood siding or unpainted wood wall shingles, and double-hung or casement window openings.

**Site Planning** - These guidelines are intended to ensure development creates an attractive residential environment for residents and visitors.



### **Building Location and Orientation**

- Parking shall be provided at the rear of the building and accessed by alleys. This will create a continuous residential street frontage that is pleasant to walk along. If parking is provided at the side of the building, it shall include a “street edge” of landscaping, walls or trellises.
- Main façades with entrance doors and windows should front the primary street.
- Owners of adjoining properties are encouraged to develop shared facilities, such as driveways, parking areas, pedestrian plazas and walkways.

### **Corner Sites**

- Buildings located on corners should include special design and architectural features that help to anchor the intersection.

### **Development Along Alleys**

- Trash enclosures, service areas, utility meters, and mechanical and electrical equipment should be located on alleys and screened from public view.
- Public access into residential development, as well as residential units with windows looking into alleys, will put “eyes on the street” and help increase security.



***Sustainable Site Development***

- Existing buildings should be reused and incorporated into new development, if possible. If reuse or incorporation of existing buildings is not possible, then building materials should be recycled.
- Solar access should be considered when site planning. Buildings should be oriented to account for winter and summer sun to maximize energy efficiency and to reduce shading on neighboring properties and public spaces.
- Non-vehicular modes of transportation should be encouraged by providing no more than the minimum number of vehicle parking spaces, creating attractive pedestrian environments and providing bicycle parking.
- Permeable paving materials should be used whenever possible. The primary purpose of permeable paving is to manage stormwater drainage on site and reduce the impact of new development on public storm drainage infrastructure.

***Pedestrian Orientation***

- Outdoor pedestrian space shall be landscaped and include appropriate street furniture to facilitate pedestrian activity. These spaces should have visibility to the street to increase security.
- Attractive, well-marked pedestrian links between parking and buildings shall be provided.

***Open Space***

- Open space, plazas, and paseos should be developed to maximize circulation opportunities between adjacent buildings. Seating areas should be provided and coordinated with shading, landscaping, and lighting.
- Private residential open space areas should be configured and designed to ensure privacy for residential uses, while also providing linkages to the public open space components of the project.

***Parking Area Design***

- Wherever possible, parking entrances should be located behind residential structures, rather than along the primary frontage, to minimize the visual impact to the street.
- Where individual garages are incorporated into projects, private streets or alley-loaded access is encouraged. The design of these structures should relate to the primary building.

***Parking Area Landscaping***

- All parking areas shall provide interior landscaping for shade and aesthetic enhancement.



**Eastern Residential District**

- If parking is provided at the side of buildings, these areas shall be buffered by landscaping, low walls and fencing. For security purposes, openings shall be incorporated into the design of buffers to provide views into the site.
- All outdoor parking areas should be divided into smaller units to decrease visual impacts associated with large expanses of pavement and vehicles, and to facilitate safe and efficient pedestrian movement between parking and residential and commercial development.
- All new projects should provide bicycle racks that are located close to the buildings and do not impede pedestrian or auto circulation.

**Building Design** - Building design guidelines are intended to ensure that development of all sizes relates to the human scale, facilitates opportunities for pedestrian activity on adjoining public streets, and contributes to the character of the Eastern Residential district.



**Massing**

- The massing of larger residential buildings shall be broken down to give individuality to units.
- Building massing should be legible as individual residences or small groups of units and called out using one or more of the following methods:
  1. Separate building volumes
  2. Window bays or balconies
  3. Porches or entrance vestibules
  4. Individual roof volumes or other roof articulations
- Architectural details commonly used in the design and construction of single-family homes, such as porches, balconies, bays and dormers, should be employed in the design of multi-family projects.

**Ground Floor Frontages**

- Main building entries or entries to upper story uses shall occur from primary streets.
- Main entrances to residential buildings should be accentuated by incorporating one or more of the following treatments:
  1. A taller mass above, such as a tower or volume.
  2. A special architectural elements, such as columns, overhanging roofs, trellises and awnings.
  3. A recessed entry or recessed bay in the façade.
- Front setbacks shall be adequately landscaped and provide entry opportunities to the residential units directly from public streets.
- Where unique use or occupancy requirements preclude the addition of

windows or entries, for example garages or parking structures, exterior walls shall be designed to provide architectural relief, or shall be screened by landscaping and pedestrian amenities, such as trellises, benches, sculpture or shade structures.

**Porches**

- Front porches should be employed to facilitate activity in front yards and to provide a semi-public transition zone between the street and the residence.
- Porches should be of a sufficient size to provide functional outdoor space.

**Windows**

- Upper story windows shall be detailed with well-proportioned architectural elements, such as sills, recesses and lintels.
- Operable windows shall be used.
- Clear glass shall be used.
- Windows should maintain consistency in shape and location across the street wall.
- Non-reflective coatings, low-emissivity glass, and external shade devices should be used for heat and glare control.

**Building Materials**

- Materials should be chosen and detailed to respect the climate and traditions of the Central Valley of California. These may include wood siding, sheet metal, stucco, tile and stone.
- Materials and detailing should be used on all sides of the building, not just the front façade.
- Natural materials should be utilized, rather than simulated materials. Where simulated materials are used, they should keep with the character and properties of the material being simulated.
- Artwork should be considered for incorporation into building design.

**Roofs and Parapets**

- Roofs should be proportionate to building mass and incorporate cornices, eaves and overhangs.
- All roof-mounted mechanical, electrical and external communication equipment, such as satellite dishes and microwave towers, shall be screened from public view and architecturally integrated into the building design.
- Landscape design, lighting and signage guidelines are found at the end of these design guidelines.



### **Downtown Landscape Design, Lighting and Signage Guidelines**

Landscape design guidelines are intended to ensure attractive public and private open spaces throughout the Downtown. Lighting guidelines are intended to ensure that the design of fixtures and the light provided contribute to the character of development, provide for pedestrian safety, and do not negatively impact adjacent development or the night sky. Signage guidelines are intended to ensure that all signs are designed and constructed to make a positive contribution to the overall character of the project and to the overall aesthetic character of Downtown. See the Ceres Municipal code for additional signage requirements.

### **Landscape Design**

#### ***Function***

- Landscaping should be used to provide an attractive setting for development, soften hard building contours, shade walkways, parking areas and other large expanses of pavement, and screen unsightly uses. Landscaping should also aid in stormwater management.
- Pedestrian entries into sites shall be enhanced with landscaping and decorative paving, trellis structures, pedestrian-scaled lighting and seating.
- Where pedestrian paths cross parking areas or driveways, the paths shall incorporate landscaping and decorative paving to define the pedestrian space.



#### ***Plants and Materials***

- Please see Figure A-2 for a list of suggested trees, shrubs and ground-cover for private development in Downtown.
- Plant species should be generally hardy and not require extensive maintenance. Plants should be drought-tolerant, while remaining attractive. Species that are native or well-adapted to the climate in Ceres are recommended, as they will generally require less water and maintenance.
- Seasonal and year-round flowering shrubs and trees should be used where they can be most appreciated, such as adjacent to walks and recreational areas, or as frames for building entrances and stairs.
- Landscaped areas shall have automatic irrigation systems installed to ensure that plant materials survive. Irrigation systems should not overspray public walks, paved areas, buildings and fences.
- Landscaped areas, including trees, paving, walls and fences shall be regularly maintained.



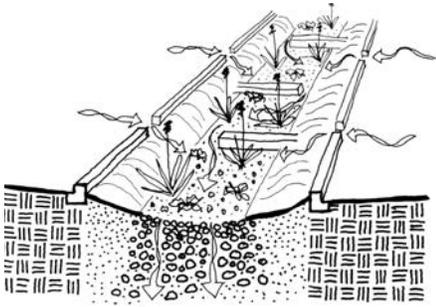
**FIGURE A-2** Suggested Planting List

<b>SYMBOL</b>	<b>BOTANICAL NAME</b>	<b>COMMON NAME</b>
<b>LARGE TREES</b>		
	Pistache chinensis Prunus 'Krauter Vesuvius' Quercus suber	Chinese Pistache Flowering Plum Cork Oak
<b>SMALL TREES</b>		
	Arctostaphylos manzanita Cercis occidentalis Cotinus coggygria	Manzanita Redbud Smoke Tree
<b>LARGE SHRUBS</b>		
	Xylosma 'Compacta' Nerium o. 'Petite Salmon' Juniperus s. 'Skyrocket' Cistus ladanifer Salvia microphylla Arctostaphylos d. 'Howard McMinn'	NCN Oleander Juniper Column Crimson-spot Rockrose Sage Manzanita
<b>SMALL SHRUBS &amp; PERRENIALS</b>		
	Epilobium californica Carex tumulicola Nandina 'Fire Power' Nassella tenuissima Erigeron glaucas	California Fuchsia Berkeley Sedge Heavenly Bamboo Mexican Feather Grass Beach Aster
<b>GROUND COVER</b>		
	Arctostaphylos 'Emerald Carpet' Myoporum parvifolium Cotoneaster dammeri	Emerald Carpet NCN Bearberry Cotoneaster

**Landscaping in Plazas, Paseos and Open Space**

- Outdoor pedestrian spaces in private development shall include appropriate outdoor furniture, such as seating, walls, trash receptacles, bike racks and other elements.
- Publicly-accessible private plazas and open spaces shall be landscaped and incorporate high-quality paving materials, such as unit-pavers, stone, concrete or tile.
- Projects should develop a comprehensive open space network that uses plazas and other open space elements to connect uses.
- Paving in plazas and open spaces should be permeable whenever possible.
- Public art should be incorporated into open space projects whenever possible.

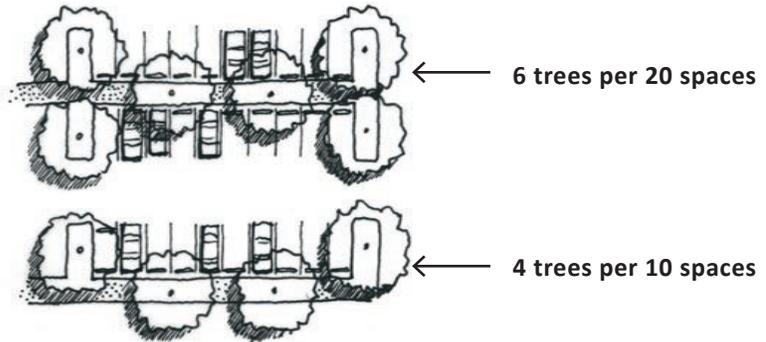




Vegetated swales can be used to detain and infiltrate stormwater runoff.

### ***Parking Area Landscaping***

- Surface parking areas shall provide landscaping for shade and aesthetic enhancement.
- Parking lots shall be landscaped with broad branching shade trees (15 foot canopy or more) at a minimum ratio of four trees per 20 parking stalls.
- Permeable surfaces for paving should be considered and used when possible.
- Drainage into swale areas is encouraged for stormwater collection and may be accommodated through design elements, such as flush curbs, perforated curbs and tree offsets.



## Lighting Guidelines

### *Function and Location*

- Exterior lighting shall be designed as an integral part of the building and landscape design. Site plans and architectural plans shall include the location of fixtures, their design, and the nature and level of the illumination they will provide.
- Lighting locations shall be concentrated at areas with security concerns, such as pedestrian paths, outdoor gathering spaces and building entries.
- The height of luminaries shall be in scale with the building and site design, and in no case shall they exceed 18 feet in height from grade.
- All outdoor lighting shall be oriented toward the ground and fully shielded to prevent light from spilling onto adjacent properties.
- Lighting should be energy efficient, and exterior lighting should be designed and specified to reduce the impact of artificial light on the night sky (“light pollution”). Fixtures should provide no more light than necessary, focus light downward rather than upward, and use “dark sky” features designed to shield the sky from excess lighting or light pollution.
- Along street frontages, lighting should cast illumination by lighting walls and architectural features on buildings rather than casting light outwards.
- Night lighting of building facades should articulate the building’s architecture and façade and should be used sparingly and in key locations.
- Structural lighting should highlight points of visual interest.

### *Service Area Illumination*

- Lighting of outdoor service, loading or storage areas should be contained within the specific yard space boundaries and enclosure walls. No light spillover should occur outside the service area and light sources should not be visible from the street or adjacent properties.





## Signage Guidelines

### *Function*

- The primary purpose of signs shall be to identify businesses and/or residences located at a specific site. If not specified in these Signage Guidelines, provisions of the Ceres Municipal Codes Section 18.42 shall govern.
- The sign's message should be limited to the business or residence name or the logo of the business or residence occupying the site.
- The information displayed on the sign shall be limited to development identification and shall not include advertising.

### *Architectural Context and Placement*

- Sign design should conform to and be in harmony with the architectural character of the building.
- Standardized or corporate signs, which do not relate to the building architecture, should not be permitted.
- Where internally illuminated lighting is used, only individual letter signs shall be permitted.
- No "can" (box type) signs with translucent plastic sign panel front with applied or painted lettering shall be permitted except for tenant logos.

### *Wall or Window Signs*

- Painted signs and letters shall present a neat and aligned appearance.
- Externally illuminated or halo lit signs are encouraged, and where used, shall have an opaque face.
- All exterior sign lights shall be downlit and shielded to direct light toward the sign and reduce glare and impacts to the night sky.
- Window signs shall not be placed in a manner which obscures primary views into and out of the storefront.
- For signs identifying hours of operation, menus, newspaper reviews and other customer information, it is recommended that these be framed, board-mounted or plastic laminated for a finished appearance.

**Projecting Signs**

- Projecting signs should be located near the front entry of a store.
- Structural supports for projecting signs should be designed so that their visual appearance is minimized, and/or coordinated with the overall design of the street wall.
- Sign fonts should be selected to provide both visual clarity and artistic expression.

**Multiple-Tenant Complexes**

- Multiple-tenant buildings and complexes shall develop a consistent sign program that minimizes the potential visual conflicts and competition among tenant signs yet ensures adequate identification for each tenant.
- Free-standing signs may include the names of major tenants.
- Monument signs should be no taller than 5 feet in height and located in the landscape, set back from the edge of right-of-way.



# A DOWNTOWN STREETScape STANDARDS & DESIGN GUIDELINES

## APPENDIX B: DOWNTOWN-SPECIFIC GENERAL PLAN POLICIES **B**

The following content is from the City's General Plan and relates specifically to Downtown:

Downtown Ceres is an important part of Ceres' small-town character and community heritage. Downtown is centrally located in the community with good access off SR 99, but is too small to serve a central major retail function for the city. Downtown includes many of Ceres' historic buildings and surrounding neighborhoods.

Policies of the plan seek to preserve and enhance this important resource by promoting increased retail, office, government, cultural, and entertainment uses in the Downtown, providing for some residential uses in second floors, and providing for parking, streetscape, and building facade improvements.

### **Goal 1.M**

To promote and support the development and maintenance of an economically viable and physically attractive downtown area.

### **Policies**

**1.M.1.** The City shall promote the commercial area downtown as a pedestrian, specialty retail, entertainment, cultural, and office and financial area.

**1.M.2.** The City shall promote use of first floor space in new buildings downtown for retail, food service, financial institutions, and other high volume commercial uses, and office and residential uses on second floors.

**1.M.3.** The City shall undertake streetscape improvements in the downtown area to give a clear and visually-appealing identity.

**1.M.4.** The City shall support programs to improve the appearance of downtown, including alley clean-ups, abatement of building code violations, redevelopment activities to reduce blight, and enhancement and consolidation of parking.

## B DOWNTOWN-SPECIFIC GENERAL PLAN POLICIES

**APPENDIX C:**  
**DEVELOPMENT STANDARDS TABLE** 

**DEVELOPMENT STANDARDS TABLE**

Standards	Fourth Street		SR 99 Gateway		Eastern Residential	
	Mixed-Use District	Civic Center/Office District	Commercial District	Commercial District	Commercial District	Commercial District
RESIDENTIAL DENSITY	Maximum dwelling units per gross acre for residential development is 40.0.	Maximum dwelling units per gross acre for residential development is 25.0.	Maximum dwelling units per gross acre for residential development is 30.0.	No residential permitted	Maximum FAR for commercial and retail uses is 0.5.	Maximum FAR for commercial and retail uses is 1.0.
FLOOR AREA RATIO (FAR)	Maximum FAR for commercial and retail uses is 3.0.	Maximum FAR for commercial and retail uses is 1.0.	Maximum FAR for commercial and retail uses is 0.5.	Maximum FAR for commercial and retail uses is 0.5.	Maximum FAR for commercial and retail uses is 1.0.	Maximum FAR for commercial and retail uses is 1.0.
HEIGHT	<ul style="list-style-type: none"> <li>Buildings shall be no more than three stories tall.</li> <li>Ground floor commercial space shall be at least 14 feet and shall not exceed 20 feet, as measured from floor-to-floor. Upper stories shall not exceed 16 feet, as measured from floor-to-floor.</li> </ul>	<ul style="list-style-type: none"> <li>Buildings shall be no more than three stories.</li> <li>Ground floor commercial space shall be at least 14 feet and shall not exceed 20 feet, as measured from floor-to-floor. Upper stories shall not exceed 16 feet, as measured from floor-to-floor.</li> </ul>	<ul style="list-style-type: none"> <li>Buildings shall be no more than four stories.</li> <li>Ground floor commercial space shall be at least 14 feet and shall not exceed 20 feet, as measured from floor-to-floor. Upper stories shall not exceed 16 feet, as measured from floor-to-floor.</li> </ul>	<ul style="list-style-type: none"> <li>Buildings shall be no more than four stories.</li> <li>Ground floor commercial space shall be at least 14 feet and shall not exceed 20 feet, as measured from floor-to-floor. Upper stories shall not exceed 16 feet, as measured from floor-to-floor.</li> </ul>	<ul style="list-style-type: none"> <li>Buildings shall be no more than three stories.</li> <li>Stories shall not exceed 14 feet, as measured from floor-to-floor.</li> </ul>	<ul style="list-style-type: none"> <li>Buildings shall be no more than three stories.</li> <li>Stories shall not exceed 14 feet, as measured from floor-to-floor.</li> </ul>
SETBACKS	<ul style="list-style-type: none"> <li>Front: Where no public utility easements interfere, development must be built to the property line for 70 percent of the site. If a public utility easement does exist, buildings should be built as close to the property line as is feasible.</li> <li>Side: None required.</li> <li>Exterior Side: None required.</li> <li>Rear Setback: 5 feet from rear property line or if an alley exists, 20 feet to alley right of way.</li> </ul>	<ul style="list-style-type: none"> <li>Front: None required, provided that no public utility easements conflict with the proposed development. Maximum front setback is 10 feet.</li> <li>Side: None required.</li> <li>Exterior Side: None required.</li> <li>Rear Setback: 5 feet, except at blocks between Third Street and Fourth Street, which shall have a setback of 20 feet to alley right-of-way.</li> </ul>	<ul style="list-style-type: none"> <li>Front: None required, provided that no public utility easements conflict with the proposed development. Maximum front setback is 25 feet.</li> <li>Side: 10 feet.</li> <li>Exterior Side: None required.</li> <li>Rear Setback: 5 feet, or 50 feet from State Route 99, whichever is greater.</li> </ul>	<ul style="list-style-type: none"> <li>Front: None required, provided that no public utility easements conflict with the proposed development. Maximum front setback is 25 feet.</li> <li>Side: 10 feet.</li> <li>Exterior Side: None required.</li> <li>Rear Setback: 5 feet, except at blocks between Fifth Street and Fourth Street, which shall have a setback of 20 feet to alley right-of-way.</li> </ul>	<ul style="list-style-type: none"> <li>Front: 10 feet.</li> <li>Side: None required.</li> <li>Exterior Side: 10 feet.</li> <li>Rear Setback: 5 feet, except at blocks between Fifth Street and Fourth Street, which shall have a setback of 20 feet to alley right-of-way.</li> </ul>	<ul style="list-style-type: none"> <li>Front: 10 feet.</li> <li>Side: None required.</li> <li>Exterior Side: 10 feet.</li> <li>Rear Setback: 5 feet, except at blocks between Fifth Street and Fourth Street, which shall have a setback of 20 feet to alley right-of-way.</li> </ul>
PARKING	Multi-family Residential: 1 space per unit for 1-bedroom	Multi-family Residential: 1 space per unit for 1-bedroom	Multi-family Residential: 1 space per unit for live-work	Multi-family Residential: 1 space per unit for live-work	Multi-family Residential: 1 space per unit for 1-bedroom units and	Multi-family Residential: 1 space per unit for 1-bedroom units and

Standards	Fourth Street Mixed-Use District	Civic Center/Office District	SR 99 Gateway Commercial District	Eastern Residential
<p>units and studios. 0.5 additional parking spaces per each additional bedroom.</p> <ul style="list-style-type: none"> <li>◆ Single-family Residential: 1 space per unit.</li> <li>◆ Commercial: 1 space for every 450 square feet of retail floor area.</li> </ul>	<p>units and studios. 0.5 additional guest parking spaces per each additional bedroom.</p> <ul style="list-style-type: none"> <li>◆ Single-family Residential: 1 space per unit.</li> <li>◆ Commercial: 1 space for every 450 square feet of retail floor area.</li> </ul>	<p>units. Commercial: 1 space for every 450 square feet of retail floor area.</p> <ul style="list-style-type: none"> <li>◆ Office: 1 space for every 375 square feet of floor area, not including areas used expressly for storage.</li> <li>◆ Medical office: See City of Ceres Zoning Code.</li> <li>◆ Bicycle Parking: Bicycle parking spaces shall be provided at a rate equal to 5 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).</li> </ul>	<p>studios. 0.5 additional guest parking spaces per each additional bedroom.</p> <ul style="list-style-type: none"> <li>◆ Single-family Residential: 1 space per unit.</li> <li>◆ Commercial: 1 space for every 450 square feet of retail floor area.</li> </ul>	<p>units. Commercial: 1 space for every 450 square feet of retail floor area.</p> <ul style="list-style-type: none"> <li>◆ Office: 1 space for every 375 square feet of floor area, not including areas used expressly for storage.</li> <li>◆ Medical office: See City of Ceres Zoning Code.</li> <li>◆ Bicycle Parking: Bicycle parking spaces shall be provided at a rate equal to 5 percent of the total required parking spaces for commercial uses and 10 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).</li> </ul>
<p>at a rate equal to 5 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).</p> <ul style="list-style-type: none"> <li>◆ Parking Reduction: Reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an</li> </ul>	<p>at a rate equal to 5 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).</p> <ul style="list-style-type: none"> <li>◆ Parking Reduction: Reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an</li> </ul>	<p>at a rate equal to 5 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).</p> <ul style="list-style-type: none"> <li>◆ Parking Reduction: At blocks between Third Street and Fourth Street, reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an</li> </ul>	<p>at a rate equal to 5 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).</p> <ul style="list-style-type: none"> <li>◆ Parking Reduction: At blocks between Fifth Street and Fourth Street, reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an</li> </ul>	<p>at a rate equal to 5 percent of the total required parking spaces for residential uses. Spaces may be in the form of racks (for more transient use) or lockers (for long-term use by employees).</p> <ul style="list-style-type: none"> <li>◆ Parking Reduction: At blocks between Fifth Street and Fourth Street, reduction of up to 20% of required parking spaces is allowed for developments that provide shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an</li> </ul>

Standards	Fourth Street	SR 99 Gateway	
	Mixed-Use District	Civic Center/Office District	Commercial District
	irrevocable access and public parking easement should be filed to ensure future owners cannot block access to shared parking areas.	shared, publicly accessible parking in the center of the block, accessed by alleys. If the 20% reduction is achieved, an irrevocable access and public parking easement should be filed to ensure future owners cannot block access to shared parking areas.	
	◆ New parking areas are strongly encouraged to be located behind buildings and shall not be located within front setbacks or exterior side setbacks.	◆ New parking areas are strongly encouraged to be located behind buildings and shall not be located within front setbacks or exterior side setbacks.	◆ New parking areas are strongly encouraged to be located behind buildings and shall not be located within front setbacks or exterior side setbacks.
PARKING LOTS	◆ Development shall maintain landscaping.	◆ Development shall maintain landscaping.	◆ Development shall maintain landscaping.
	◆ Native and other drought-resistant or drought-tolerant landscaping shall be provided consistent with City of Ceres Water Efficient Guidelines and Standards.	◆ Native and other drought-resistant or drought-tolerant landscaping shall be provided consistent with City of Ceres Water Efficient Guidelines and Standards.	◆ Native and other drought-resistant or drought-tolerant landscaping shall be provided consistent with City of Ceres Water Efficient Guidelines and Standards.
LANDSCAPING	◆ Development shall include low-impact development (LID) stormwater collection and treatment measures to control runoff flow and volume, such as vegetated swales, vegetated buffer strips, flow-through planters, bioretention areas, infiltration trenches, and other	◆ Development shall include low-impact development stormwater collection and treatment measures to control runoff flow and volume, such as vegetated swales, vegetated buffer strips, flow-through planters, bioretention areas, infiltration trenches, and other	◆ Development shall include low-impact development stormwater collection and treatment measures to control runoff flow and volume, such as vegetated swales, vegetated buffer strips, flow-through planters, bioretention areas, infiltration trenches, and other similar

Standards	Fourth Street Mixed-Use District	Civic Center/Office District	SR 99 Gateway Commercial District	Eastern Residential
	similar measures.	similar measures.	similar measures.	measures.
◆	If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.	If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.	If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.	If retaining existing trees in new development proves infeasible, new trees should be planted at a one to one (1:1) replacement rate. Replacement trees should be a minimum box size of 24". New development may also choose to pay a fee in lieu of direct replacement. Collected fees should fund street tree installation. The fee to be collected should be determined by the Public Works Director or other appropriate City designee.
ACCESS	◆ There shall be vehicular access from a public street or alley to off-street parking. ◆ All ingress and egress to and from public property shall be in a forward motion. ◆ If a single development encompasses more than 40 percent of frontage on a single block, a publicly accessible pedestrian paseo leading to rear parking areas must be provided.	◆ There shall be vehicular access from a dedicated street or alley to off-street parking. ◆ All ingress and egress to and from public property shall be in a forward motion. ◆ If a single development encompasses more than 40 percent of frontage on a single block, a publicly accessible pedestrian paseo leading to the rear parking areas must be provided.	◆ There shall be vehicular access from a dedicated street or alley to off-street parking. ◆ All ingress and egress to and from public property shall be in a forward motion. ◆ If a single development encompasses more than 40 percent of frontage on a single block, a publicly accessible pedestrian paseo leading to the rear parking areas must be provided.	◆ There shall be vehicular access from a dedicated street or alley to off-street parking. ◆ All ingress and egress to and from public property shall be in a forward motion. ◆ If a single development encompasses more than 40 percent of frontage on a single block between Fourth Street and Fifth Street, facing Fifth Street, a publicly accessible pedestrian paseo leading to the rear parking areas must be provided.

<b>FRONTAGE STANDARDS</b>			
<b>Standards</b>	<b>Whitmore Avenue Overlay</b>	<b>North Street</b>	<b>Historic Fifth Street</b>
<b>RESIDENTIAL DENSITY</b>			
<b>FLOOR AREA RATIO (FAR)</b>	Maximum FAR for commercial and retail uses is 3.0.		
<b>HEIGHT</b>	<ul style="list-style-type: none"> <li>◆ Buildings facing Whitmore Avenue shall be no more than three stories.</li> </ul>	<ul style="list-style-type: none"> <li>◆ The portion of buildings within 20 feet of the property line abutting Fifth Street shall be no more than two stories.</li> </ul>	<ul style="list-style-type: none"> <li>◆ Buildings shall be set back an average of 20 feet from Fifth Street. Setbacks for individual building elements are encouraged to vary from 10 to 30 feet.</li> </ul>
<b>SETBACKS</b>	<ul style="list-style-type: none"> <li>◆ Buildings shall be set back a minimum of 10 feet along the Whitmore Avenue frontage.</li> </ul>	<ul style="list-style-type: none"> <li>◆ Buildings shall be set back a minimum of 10 feet from North Street</li> </ul>	<ul style="list-style-type: none"> <li>◆ Development that is adjacent to Fifth Street, but does not have its primary frontage on Fifth Street, shall provide significant landscaping or a publicly accessible open space between the building edge and Fifth Street.</li> </ul>
<b>LANDSCAPING</b>		<ul style="list-style-type: none"> <li>◆ Landscaping treatment shall be provided within setbacks adjacent to North Street.</li> </ul>	<p>In order to preserve the visual character of historic resources along Fifth Street, the following standards shall apply to development on parcels abutting historic resources.</p> <ul style="list-style-type: none"> <li>◆ Subject properties: Parcels abutting Whitmore Mansion (APN # 127015017) and the Whitmore Home and Museum (APN # 127017035).</li> <li>◆ Setbacks: Parcels to the side and rear of a historic structure shall be set back a minimum of 20 feet from the lot line abutting the historic structure's lot line. No parking shall be allowed in this setback</li> </ul>
<b>ADDITIONAL HISTORIC STANDARDS</b>			

Standards	Whitmore Avenue Overlay	North Street	Historic Fifth Street
			<p>and it shall include landscaping appropriate to the historic character of adjacent properties.</p> <ul style="list-style-type: none"> <li>◆ Height: Within 30 feet from the lot line abutting the historic structure's lot line, height of new development shall be no greater than the height of the abutting historic structure.</li> <li>◆ Landscape buffer: Within 30 feet from the lot line abutting the historic structure's lot line, height of new development shall be no greater than the height of the abutting historic structure.</li> </ul>

C DEVELOPMENT STANDARDS TABLE



